



HONEYSUCKLE PUBLIC DOMAIN STRATEGY



A collaboration between
Honeysuckle Development Corporation,
Department of Urban Affairs and Planning,
Newcastle City Council,
Major stake holders and consultants
Pittendrigh Shinkfield and Bruce.

Final - November 2000

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EXECUTIVE SUMMARY



Newcastle Icons: Nobby's and Fort Scratchley

The Honeysuckle Public Domain Strategy is a Strategy about information and process, a rich tool kit of ideas. As such it is to be a briefing document for future development, where what has been uncovered and expressed will not only be informative in a public open space sense, but will pervade and inform future built outcomes. In a fluid way it is to frame the new meaning of this place, adapted and discovered through time.

At the initiative and request of the Honeysuckle Development Corporation, Pittendrigh Shinkfield and Bruce were engaged to prepare a Public Domain Strategy following Council's adoption of DCP40 City West. The Strategy:

- provides the setting for development;
- shapes the public realm;
- identifies the heart and soul of the place, Honeysuckle; and
- is an extension of the Honeysuckle Development Corporation's fundamental objective of providing Honeysuckle with a breath of new life by implementing its vision of creating 'Newcastle's home by the harbour'.

The Strategy is set out sequentially in the order of the study and attempts to reflect the dialogue and attitudes of the moment.

The context is set out in Section 1.0 Introduction where a number of key frameworks are established.

Firstly, the review of the physical setting of Newcastle City highlights two concepts:

- the city maintains a local identity on a global level; and
- there is a strong link between technological innovation and industrial process.

Secondly, the planning landscape within which this Strategy needs to function is framed by the primary statutory document DCP40 City West: Unlocking the Potential. It is upon this document that the Public Domain Strategy has been developed and will work along side once adopted.

Thirdly, the essence of a successful and meaningful public domain is explored. It has been discovered that the key for the Honeysuckle site lies in the identification and appreciation of the power of two forces:

- collective memory of the past; and
- hope for an ecologically sustainable future.

Generally, the Strategy attempts to establish a public domain framework that is richly expressive of the collective memory through appropriate historical referencing and intrinsic physical processes occurring at Honeysuckle.

What stands out, as a distinctive attribute of the Honeysuckle site, is the characteristics of change that have pervaded all areas of its history and development. Honeysuckle is a complex blend of history, environmental changes, development and planning controls and community ideals. As such, an understanding of the historical layering and environmental constraints of the site is

fundamental to an appreciation of the Strategy's underlying concept and recommendations. Consequently, historical and information research became a vital component of the strategy.

Section 2.0 Review and Research explores the rich and diverse environmental factors relating to Honeysuckle. It uncovers the sequence of change that is seen as a defining characteristic of this site. The Strategy concludes with a summary of the community consultation outcomes that provide a clear picture of Novacastrian's ideals today, their desire to be involved in the development process and displays the depth of pride they feel for their city.

The overall direction for the Strategy is identified in Section 3.0 Opportunities. Fundamentally, the aim of the Strategy is to uncover and retell the fascinating story of Honeysuckle's history through the development of the public domain. Key thematic concepts were developed at the broadest scale to achieve this for the site. These include:

- Foreshore edge transformation;
- Fractures, fingers and connectors;
- Transition and connection; and
- Repetition.

The identification of key thematic concepts is designed to inform the redevelopment process and in particular the development of a meaningful and layered public domain.

For the purpose of understanding the sequence of change and for implementing a phased development program, Honeysuckle was divided up into thematic precincts. These include:

- Merewether Wharf Precinct;
- Urban Entertainment Precinct;
- Lee Wharf Precinct;
- Worth Place Park;
- Cottage Creek Precinct;
- Wickham Urban Village;
- Honeysuckle Drive; and
- Connectors and View Corridors.

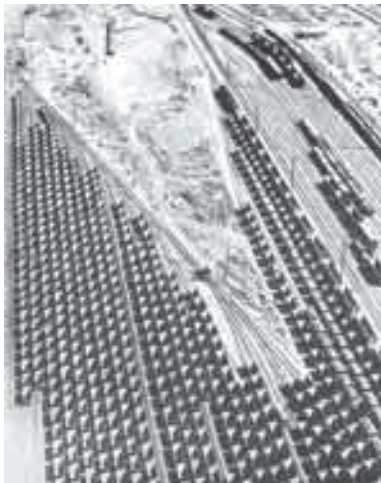
Within the community consultation program, there was opportunity to test earlier concepts and to question earlier planning decisions that had been made in the absence of historical reference. Due to the advanced nature of development interest for the Urban Entertainment Area, this precinct was tested. The existing structure plan as outlined in DCP40 was tested against three options developed on three key objectives. Two key issues were raised though this exercise and pervaded all future precinct development discussion. These were:

- Civic connection between Honeysuckle and the Civic area; and
- Implementation of light rail as the preferred transport option.

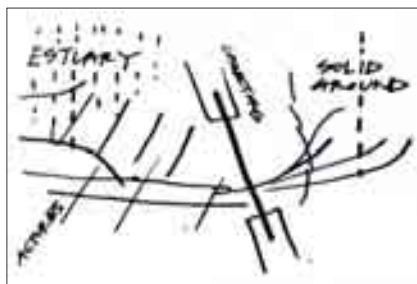
The history of Newcastle demonstrates Honeysuckle as being a place of energy, both natural and human, creativity and technical innovation and suggests that the public domain becomes a place where the fusion of art and science is clearly evident and indeed a determining characteristic of the sense of place. To



The Tree of Knowledge



Nest of coal wagons, c.1967.
Source: Barney & Callen 1989



Sketch ideas

this end an inventory of ideas has been developed by artist Nola Farmen to demonstrate how conceptual ideas could be implemented as part of the broader public domain.

Section 4.0 Public Domain Precincts develops detailed strategies for each thematic precinct. Each precinct outlines a clear direction in the following areas and is supported by a suggested pallet of materials and plant species:

- Vision Statement;
- Activities;
- Objectives and Principles; and
- Spaces.

The Public Domain Strategy concludes with Section 5.0 Implementation. Whilst the core fabric of the public domain will be incrementally stitched into place over time, as the various development precincts come on line, the Strategy has identified several strategic public domain projects as being key to stimulating the necessary momentum to sustain development over the coming years. These projects should be prioritised and budgeted for implementation over the next five years and include:

- Worth Park Place and Foreshore linkages;
- Urban Entertainment Center Environs;
- Honeysuckle Drive;
- North South connectors; and
- Cottage Creek.

The implementation of these key public domain projects will prove to be significant in influencing the status of the political, social, environmental and economic realms of Honeysuckle. Essentially the implementation of key public domain projects and a quality-integrated public domain represents the opportunity for a win-win situation for all key stakeholders.

In order to maintain and continue the endorsed vision the Honeysuckle Development Corporation, must start initiating the process, which is perhaps the hardest task. In this sense the public domain should be initiated with a significant piece such as Worth Place Park, a piece which will stimulate the imagination of potential developers, and the hearts of the people of Newcastle.



Energy biproducts
Photograph by Allan Chawner.
Source: Moore & Ostwald, 1997



The construction of Nobby's Breakwater, 1820.
Source: Turner 1997.

The modification of Nobbys Island throughout the first half of the nineteenth century (today its height is only half that of the original) is strongly symbolic of the way Europeans appropriated Aboriginal land and changed it for their own purposes. In the 1820's the missionary Lancelot Edward Threlkeld wrote that "Manual labour is now employed to fill up the space betwixt the island and the mainland so as to form a breakwater for the protection of the harbour at Newcastle, and a great part of the top of Nobbys Island has been taken down without reaching the monster kangaroo said to dwell within the rock".

Jo Hanley 1997

1.1 STUDY SETTING

At the initiative and request of the Honeysuckle Development Corporation, Pittendrigh Shinkfield and Bruce were engaged to prepare a Public Domain Strategy which from the outset was understood to be a document concerned with providing the setting for development in Honeysuckle, shaping the public realm and identifying the heart and soul of this place.

It was always intended that this report, this strategy, would be about process and information, a tool kit of ideas. It was to be a briefing document for future development, where what was uncovered and expressed would not only be informative in a public open space sense, but that the thinking would pervade and inform future built outcomes. In this fluid way the new meaning of this place could be adapted and discovered.

It must be stated that this document is not about light fittings and street furniture, nor is it a “masterplan”. Many of these reports have already been written and shelved or written and archived, their prescriptive approach proving inflexible in the context of the rapidly changing face of our urban requirements and they fall short in terms of development expectations. For Honeysuckle six years of planning, assessment, reporting, masterplanning and site engineering had culminated in a divided view on what Honeysuckle was to become, where connections to the past were shaky, and the vision to the future cloudy.

Honeysuckle is a complex blend of history, environmental change, development and planning controls and community ideal. It is in fact an exciting site which has the potential to tap into the collective memory of Novocastrians, and into the memory of all those who have visited and participated in Newcastle throughout its history and who have been involved in its tradition of enormous change.

This is a site where what was once sea is now land, and what was land is now sea, where what was once a shallow meandering creek rich in food source and habitat is now a deep water harbour devoid of vegetation and wildlife, and where what was once an intensity of activity and energy exchange at the wharf is now a deserted and decaying edge.

It is this shifting, this wrestling between urban and natural process to claim and reclaim which has captured our attention and brought focus to the public domain as part of the next wave of change and as part of redefining this place. This next wave is welling up on the foundation of the past, breaking in the present, and washing across blurred boundaries to provide the framework for moving forward into the future.



But what of its hidden history- those histories of Newcastle which have been lost, neglected or ignored? We cannot presume to know the 'truth' about the Awabakal. What we can be sure of is that the absences in the landscape are just as telling. An analysis of 'what is there' and 'what is no longer there' then becomes a source for articulating the changes that have occurred over time.

Jo Hanley 1997



View west across the site commencing at Merewether Street
Source: Honeysuckle Development Corporation



Aerial photograph 1993. Source: Land Information Centre

1.2 HOW TO USE THIS STRATEGY

FORSBACKA

*Meeting in space and time
Place*

*Buildings remain
Activities change*

*Smelting no more
Gone is the ore*

*Restore
Implore*

*Explore more uses
Create new ways*

*Lives to live
Capturing the best of the past*

*Fulfilling inner needs
More than just a space*

*To live
A feeling*

*A natural place
New ways in old spaces*
Joy Llewellyn-Smith, 1992

An understanding of the historical layering and environmental constraints of the site is fundamental to an appreciation of the Strategy's underlying concept and recommendations. Consequently rather than placing historical and research information into an appendix this vital aspect of the study has become part of the body of the report.

Even if not committed to the whole report, we would encourage you to at least scan through the assemblage of images and ideas, with the hope that you will become actively engaged in the fascinating journey, a journey in which we have had the privilege of being involved.

Significantly, this document provides

- an review of the current physical and planning contexts;
- review and analysis of research relating to the physical environment and the characteristics of change relating to Honeysuckle;
- an overall urban design framework and interpretation of the information gathered; and
- clear implementation direction.

It is intended that this document will be used for the following purposes:

- as an extension of the current City West Development Control Plan 40;
- as a design framework for developers; and
- in the review of development proposals.



View of the eastern most sector of the site.
Source: Honeysuckle Development Corporation



View towards Civic from the existing boat harbour

1.3 OWNERSHIP

The success of Honeysuckle as a public place is dependant upon the level of community 'ownership'. As a result, the process of forming a Strategy encouraged dialogue between the public, public authorities and commercial interests. The Strategy is an expression of this dialogue, where disparate concerns have been voiced, meaning and aspirations exposed, difficulties overcome and a view of the future possibilities for Honeysuckle visualised.

1.4 THE TEAM

The structure of the team and its ability to work in both an interdisciplinary and intradisciplinary manner has been instrumental to the study outcome.

The Strategy is genuinely the result of a collaboration between the following members:

- Angus Dawson (HDC) - Project Director
- Julie Rich (HDC) – Project Manager
- Jon Shinkfield (PSB) - Team Leader
- Mike Milligan (PSB) - Concept development
- Annabel Stanton (PSB) - Research, Reporting and Community Facilitation
- Mark Blanche (PSB) - Environmental Planning
- Nola Farman - Interdisciplinary Artist
- Angus Bruce (PSB) - Cost control



View across to The Basin, Carrington

It should also be noted that the community, stakeholder group representatives and Newcastle City Council have played a vital role in the development of the Strategy, contributing to our knowledge of the area and to understanding the particular issues at hand. We have greatly appreciated their ongoing input.



Newcastle Icons: Nobby's and Fort Scratchley



The Tree of Knowledge



Newcastle City Skyline

1.5 AIMS AND OBJECTIVES



Mike Milligan (left) and Nola Farman (right) with Jon Shinkfield on board the Pilot boat.

The public domain consists of shared spaces and areas such as the waterfront streets, laneways, parks, plazas, promenades and the built edges that form those spaces.

City West DCP 40

It is the fundamental corporate objective of the Honeysuckle Development Corporation to provide a breath of new life by implementing its vision of creating 'Newcastle's home by the harbour'. A new foreshore place that reconnects the working city to the working harbour, and opens up the foreshore for public use, connecting Linwood/Wickham in the west through to Nobby's in the east. Honeysuckle is to be a place where people live, work and play. A place where every person has access to one of the great industrial harbours of the world.

Honeysuckle is a place waiting to happen; where there is an underlying sense of anticipation, where that the breath of new life for this post industrial foreshore site is imminent. Although the industrial bones of the site have long since vanished, the ghosts of a once bustling vibrant foreshore remain and are somewhat restless.

It is the fundamental vision of the Strategy to develop a rich, coherent tool kit of ideas to guide the development of the public domain at Honeysuckle – Newcastle's home by the harbour. To become a home, a place must be lived in and adorned with the intrinsic 'stuff' of those that call it home. This is sometimes called sense of place, other times the collective memory. What it is though is the soul of a place, the stuff that distinguishes a place from another. With so much waterfront redevelopment occurring around the globe, it is imperative that the public domain of Honeysuckle is imbued with a soul that is truly Novacastrian at this point in time. The Strategy aims to avoid the generic, the global, the homogenous and attempts to uncover the layers of a rich and dynamic past as a foundation for fresh and innovative development projecting Newcastle into the new millennium.

In this context, this Strategy is designed to ensure that Honeysuckle becomes another vibrant urban district, accessible and enjoyed by everyone, through achieving the following aims:

- Interconnection between existing foreshore public space and urban context;
- Maximise accessibility and permeability;
- Create diversity within Honeysuckle;
- Reveal landscape character and reference sequence of change;
- Enhance human scale environment and promote interaction;
- Stage development to suit changing needs; and
- Catalyse existing studies to form a coherent and integrated guide to development.

The Public Domain Strategy is an ongoing part of the broader planning framework within Newcastle City Centre. Consultation of previous work has included the following key documents;

- City West Development Control Plan 40
- Honeysuckle Point Heritage Study
- Urban Design Plan for the Newcastle Civic Area
- Cottage Creek Precinct, Honeysuckle Development Post-Charette Concept Plan.
- Wickham Urban Village Concept and Strategy Study, and
- useful insights gleaned from Hidden Newcastle



Lee Wharf: The restored, the decaying, the living foreshore

1.6 NEWCASTLE: THE MILLENIUM CITY

Honeysuckle is pivotally positioned as one of two key sites that will project Newcastle into the new millenium. The other is BHP and associated Steel River Project. The closure of the BHP steelworks (except for Rod and Wire Manufacture) is for locals a symbolic turning point for the city. For some it may be a relief as the city can now move forward in its focus on becoming a post-industrial city in the technological age in the new millenium.



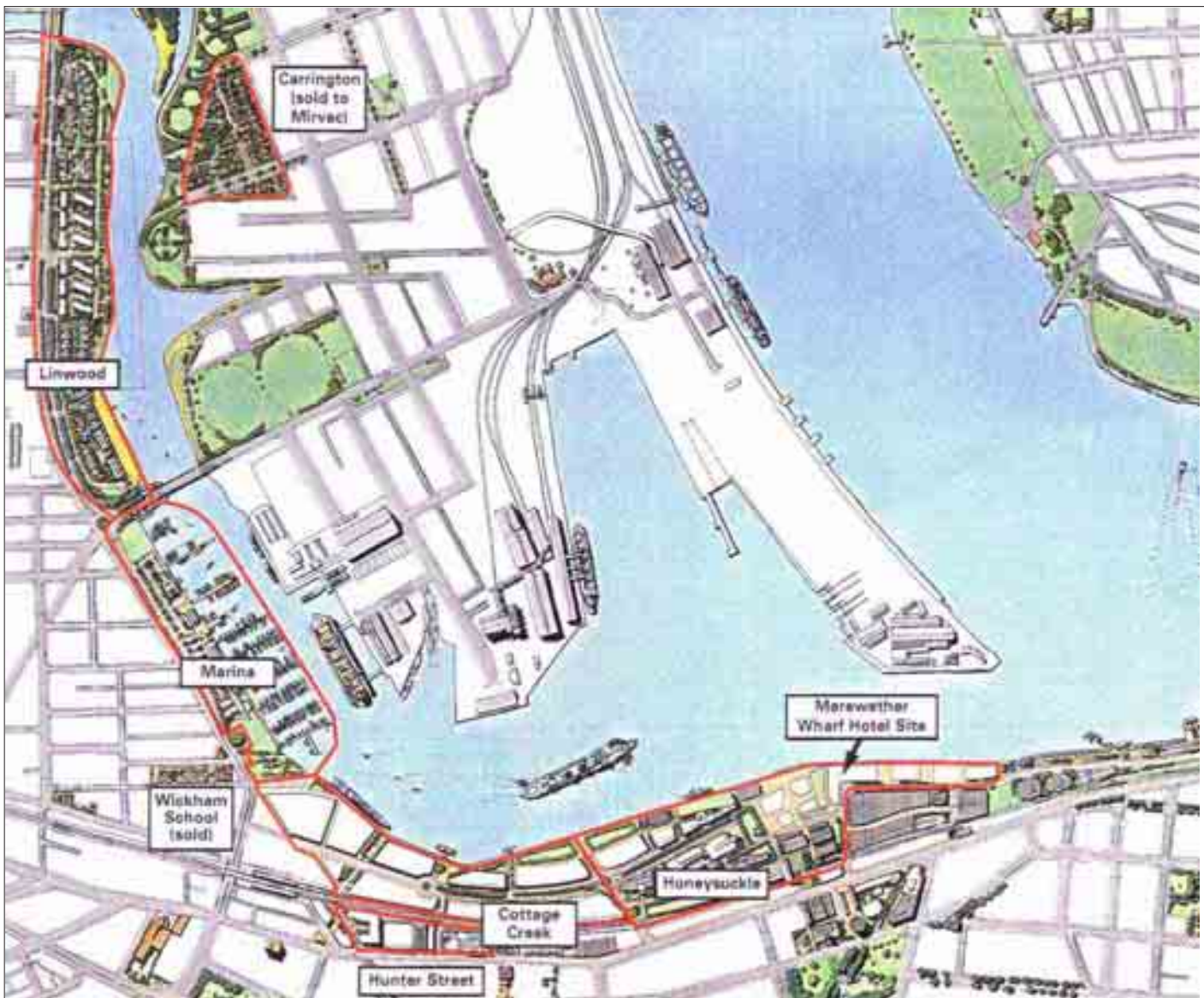
The team at work from left to right, Nola Farman, Julie Rich, Annabel Stanton, Mark Blanche and Mike Milligan's hand.

Honeysuckle offers Newcastle the opportunity to create a sustainable post-industrial waterfront community that is intrinsically local in its contextual connections (both physical and referential) yet continues to reach out and connect globally – in much the same way as the industrial port operations exported goods and resources to the global market of the Colonial era. Likewise, Steel River offers a complimentary opportunity to develop a place for progressive, hi-tech, sustainable industry to continue to undergird Newcastle as a place of export and technological innovation.

Newcastle is well known as an industrial city, like its namesake in England. However, it is important to make the connection of technological innovation as part of the significant industrial progress achieved in Newcastle over the last century. It is this innovative and industrious spirit that underlies the current intellectual, artistic and political move towards an ecologically sustainable city. The energy of a place that has produced so much in the last century is evident today in its people, in particular key decision-makers. Newcastle has infrastructure to become a global leader in sustainable futures for the new millenium. Forced to face the impact of a global economic rationalization it is using these associated crisis as an opportunity to shape its future. Already, projects such as Illuminating Newcastle, The Pathways to a Sustainable Hunter and technologically innovative work undertaken by local 'youth' suggest the way forward is to develop sustainable local communities that can participate (and indeed retain their identity) on a global level.



"Newcastle's home by the harbour"
Source: HDC Marina Development Opportunity Brochure



Current Honeysuckle Masterplan as prepared by HDC

1.7 PUBLIC DOMAIN OVERVIEW

The public domain can be defined as the publicly owned and accessible places and spaces that occupy the interstitial zones between private sector developments. The public domain is the place where a sense of place, a sense of collective ownership and a place of memory can be created. Honeysuckle with its current green field state offers a unique opportunity in terms of creating a public domain as opposed to reinvigorating an existing public domain.

The strategy recognises that certain intrinsic qualities of Newcastle as a place distinguish it from other places. In particular the industrial history and stable community fabric have combined to produce a powerful collective memory within Newcastle. Equally significant has been the gravitation towards the principles of ecological sustainability as a community response to the post-industrial forces now transforming the economic, social and physical landscape of the city.

The key to a successful and meaningful public domain at Honeysuckle lies in an appreciation of the power of these two forces - collective memory of the past and hope for an ecologically sustainable future. The Strategy attempts to establish a public domain framework that is richly expressive of this collective memory through appropriate historical referencing and of the intrinsic physical processes occurring at Honeysuckle.

Comparing Cities - Newcastle and Fremantle

Post Industrial Port Cities

To understand the current post-industrial processes affecting significant economic, social and physical change across Newcastle, it is useful to review the impacts of post-industrial changes to Fremantle. The two cities have significant comparable qualities:

- Deindustrialising regional industrial ports over the last twenty years;
- Retention of their intrinsic architectural qualities largely due to indifference and being overlooked during the modernization of metropolitan cities in the 1960's and 70's; and
- Significant events and external shocks, in the late 80's, have redefined their respective urban foundations;
 - Fremantle – Americas Cup Defence 1987;
 - Newcastle – Earthquake 1989.

These events were somewhat disparate, yet had similar impacts on their respective cities:

- neither event was anticipated or requested by the locals,
- both events brought global recognition to their cities,
- the quantity of post-event investment that flowed into the cities was roughly equivalent, and
- both cities underwent “searching, painful struggles over the built environment and the balance between development and heritage”.

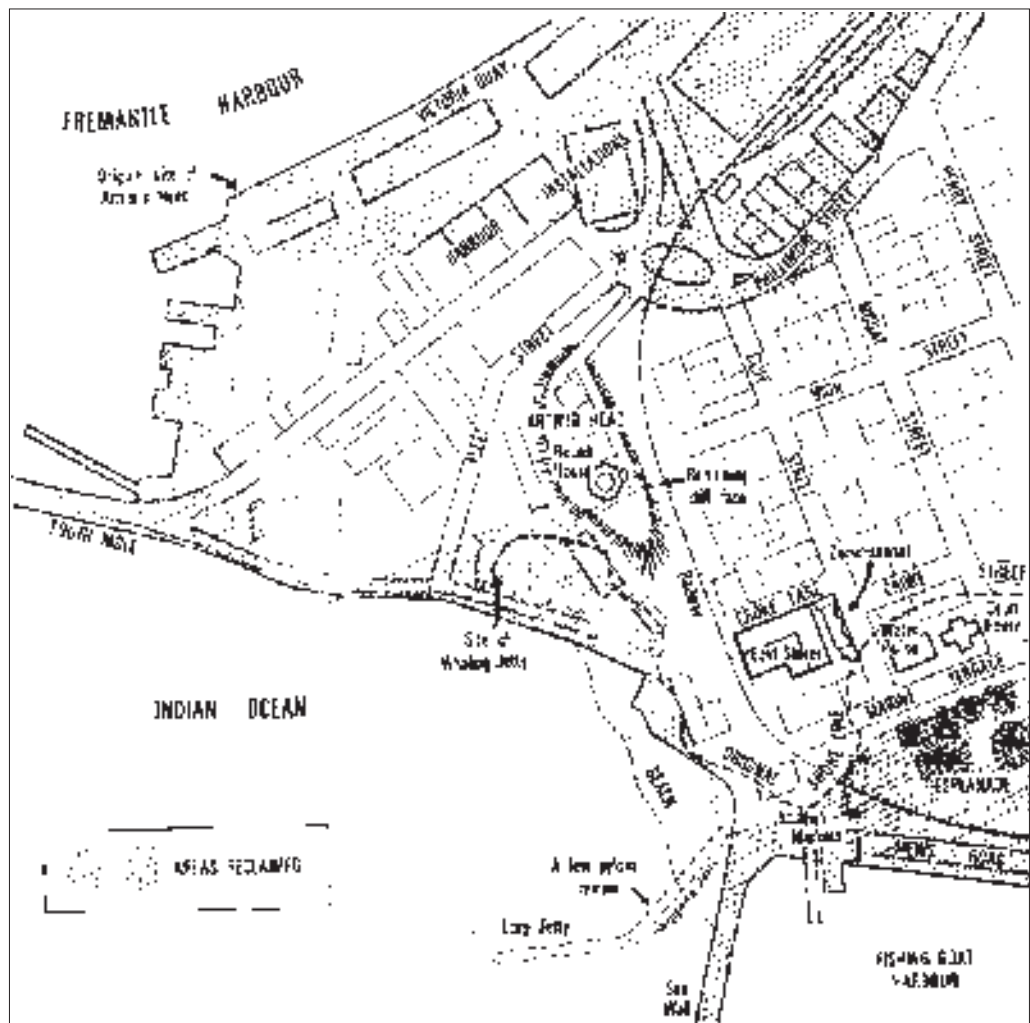




Fremantle 1987:
America's Cup Spectators, same place but different event
Source: Rowett and Baxter, S. 1997

... the continuous development of public space relies upon the design of new negotiation processes within the frame of existing political institutions.

Conan 1992



Fremantle's changing shoreline tells a similar story to Newcastle Harbour
Source: Courie, S and M

Key Issues

Although the funding paradigms were different in the two cities, the physical, social and economic impacts were similar.

Honeysuckle represents a significant morphological impact on the port city of Newcastle, in much the same way as the America's Cup program impacted Fremantle. The America's Cup redevelopment program was largely funded with public funds. The Honeysuckle redevelopment program focuses on an entrepreneurial approach. It is this latter approach that needs to be carefully managed, particularly within the Urban Entertainment Centre precinct where there is the risk of creating a privatised public domain.

Critics of both programs identify displacement, destruction and alienation as critical impacts of post-industrial development program paradigms.

Critical concerns with Honeysuckle that have been previously expressed by leading public commentators include:

- displacement away from city centre focus on CBD and historic east end of town, particularly associated with potential closure of rail between City and Civic;
- destruction of memory, both seen and unseen, through replacement of industrial fabric/legacy with post-modern theme-park type development;
- alienation of the city's poor through this type of development, provision of amenity and services for a "relatively affluent population".



Fremantle c1850's: "Somewhere different to go on a Sunday"
Source: Bram, P. 1996

Public Domain Clues

Certain clues for key public domain strategy attributes can be drawn out from this comparative study. They include:

- ensuring a highly accessible and equitable public place that connects the 'old town' eastern core and new western waterfront edge;
- maximise the potential of contemporary architecture to represent current architectural and cultural values and create a vital, contemporary public place;
- develop a truly Novacastrian place through the development of appropriate historical referencing, physical narrative and trafficking in memory to validate its unique past (in particular its place as the engine room of colonial growth and prosperity to the more recent 1989 earthquake) as a reference point for future development.



Curvilinear constructed forms contrast with the remnant linear projections



Direct contact with the water's edge - a highly unusual condition in New York City

The designers created a site-specific language whose formal cues came from the waterfront's curved and shifting contours.

**Deborah Maston Landscape Architecture
1999 Vol 89/1**



The "stabilized ruin" of the signature gantries with layering of constructed relic elements

Comparing Places



Provision for foreshore activities

Post Industrial Waterfront Park: Gantry Plaza, Queens West, NY, USA

Gantry Plaza illustrates a contemporary approach to creating new waterfront places that are involved with a sense of collective memory of past industrial processes as well as a sense of the new, the modern, a hope for the future. This is consistent with the vision for Honeysuckle.

Across from Manhattan's East Side, Gantry Plaza is the first phase of redevelopment on the western most point of Long Island. The gantries have been retained as important historical artefacts and powerful signature design elements for the park. They successfully create a powerful, mediating middle ground in the view across to the Manhattan skyline.

The retention of four timber pier structures allow access 100 feet out into the harbour and create an appropriate linear contrast to the curvilinear foreshore circulation system. Each of the piers have been designed to provide different spatial and social experiences along the foreshore edge.

Further, direct access to the harbour edge has been created through detailed granite block terraces cut into the stabilised foreshore edge. This is a highly unusual event in New York and provides clues for opportunities in Honeysuckle.

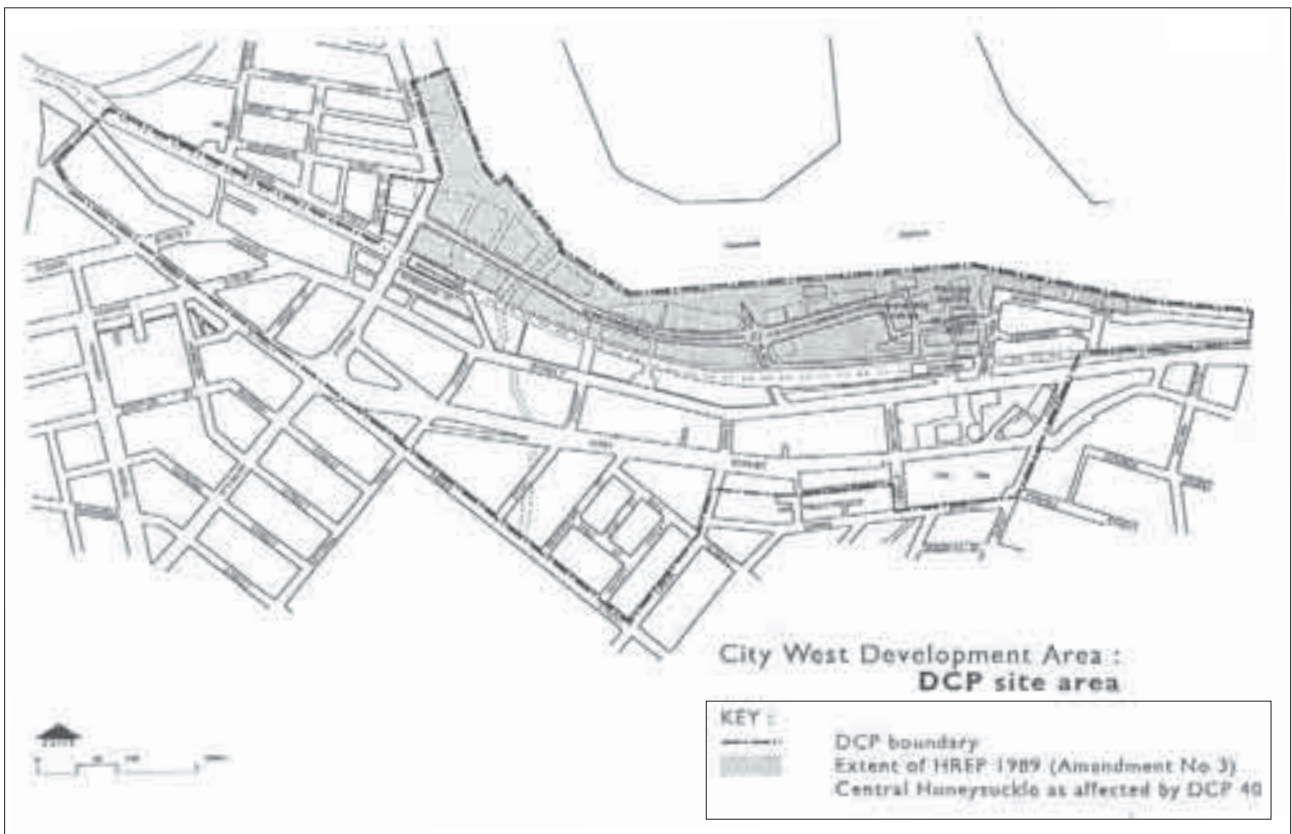
Clever use of appropriate materials and detailing evoke the energy that must have existed when movement of freight was this site's only purpose. In this same context there is a strong similarity with Honeysuckle, a site with similar energies and synergies – collective memory and ecological sustainability.

Elegantly executed site furniture and material transitions contribute to a sense of dynamic poise evident in the plaza as a whole.

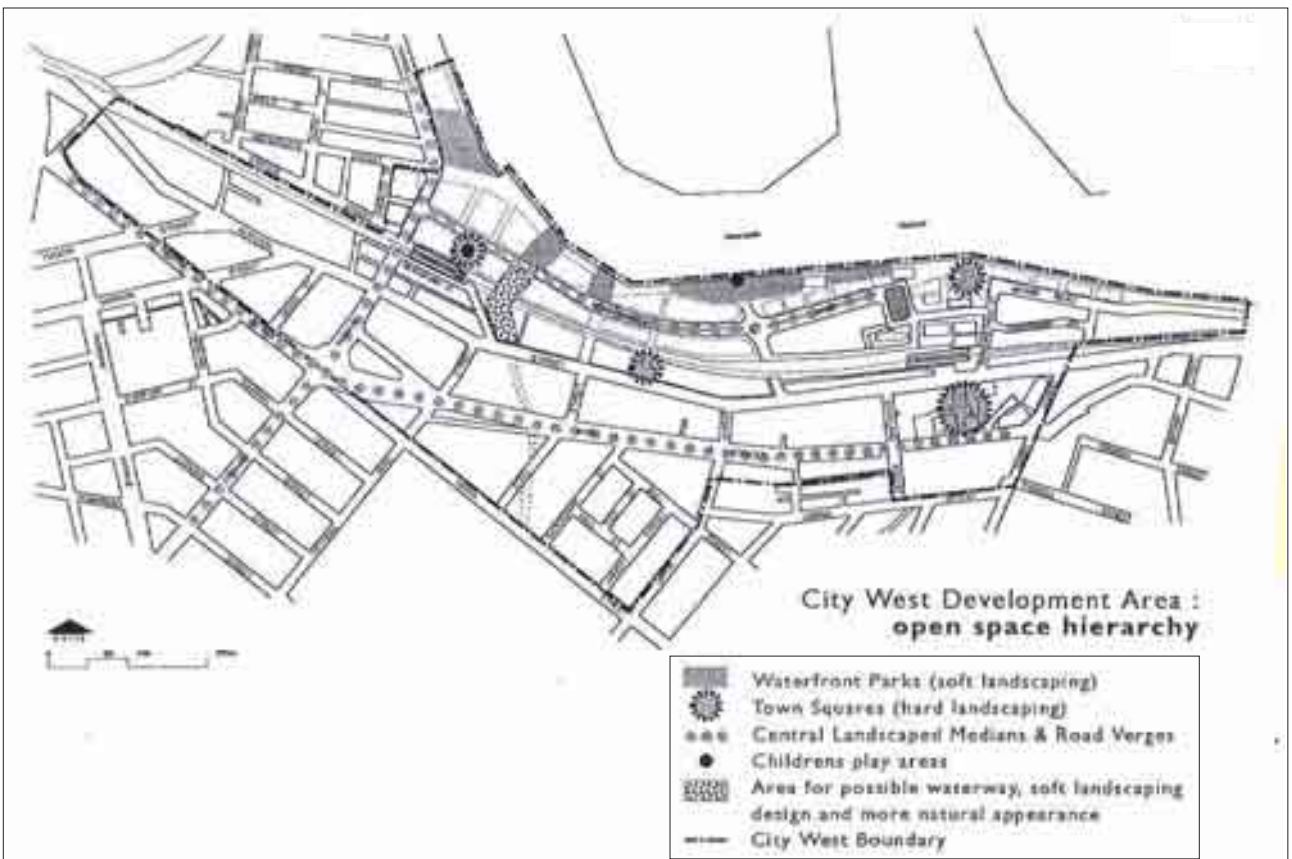
Deborah Marton, Landscape Architecture 1999 Vol 89/1



Customised furniture, fixtures and detailing enhance the place as a whole.



Honeysuckle occupies nearly 50% of the City West Development Area



The development of an interconnected open space system is a key objective of the DCP

VISION

City West is the gateway for a revitalised City Centre with a wide range of mixed use activity around the clock. The area will be a safe and accessible place to live, a place for economic and social exchange and a place for employment.

City West DCP 40

STRUCTURE

The evolution of the new City West structure will be achieved by:

- *Establishing or reinforcing centres of mixed use activity along Hunter Street, around Wickham and Civic railway stations and at Worth Place and Steel Streets.*
- *Establishing new North-South connections to help reinforce Hunter Street's mainstreet role and support key centres along it whilst also emphasising the connections between the mixed use area that will develop to the north and south.*
- *Reinforcing streetscape/public domain character for the major pedestrian, cycle and vehicular access routes which bisect City West (i.e. the waterfront promenade, Honeysuckle Drive, Hunter Street, King Street and existing and proposed new North-South linkages).*
- *Reinforcing the cultural axis and transition zone between City West and City East, running from the Civic Workshops and waterfront through Civic Park to the Art Gallery.*
- *Defining and reinforcing a new movement network to support pedestrian and cycle movement and provide strong linkages between the various activity nodes, neighbourhood centres.*

City West DCP 40

GOALS FOR HONEYSUCKLE AND ENVIRONS AREA STRATEGY

- *Revitalising Newcastle's business district*
- *Developing a mix of affordable and other housing choices*
- *Improving employment opportunities and diversifying the economic base of the city and the Hunter Region*
- *Improving the quality of life in the city centre and inner suburbs by making it a more attractive place to live and increasing public access to the harbour foreshore and,*
- *Encouraging use of public transport and other public facilities in inner city areas*

Wickham Urban Village Concept and Strategy Study 1995

DCP 40 City West: Unlocking the Potential is the primary statutory document upon which the Public Domain Strategy has been developed. The detail development of the public domain should demonstrate an understanding and appreciation of both the intent and content of the City West DCP. The key sections of the DCP can be summarised as follows:

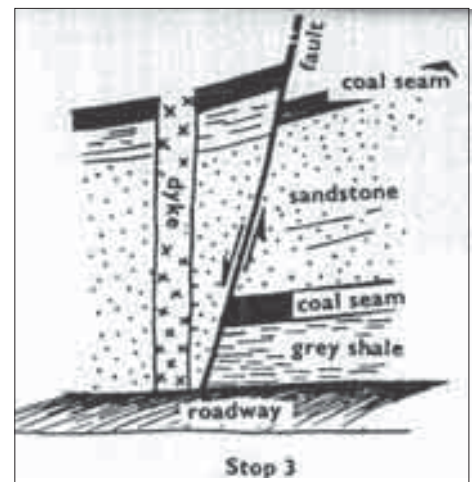
City Structure covers the important features that will help make City West a legible place, integrated with the fabric of the City. Features include defining major entry points with gateway elements, conserving, protecting and enhancing existing visible city landmarks, creating new landmark development opportunities, and creating major and minor view corridors to frame views to the harbour and key landmarks such as Nobby's Headland and the Tree of Knowledge.

City Form relates to the three dimensional character of City West including massing, form and height of buildings in relation to the topography, street scale, and the intensity of development in different areas. This section also addresses issues such as vehicular access management, car parking, flooding and geotechnical constraints.

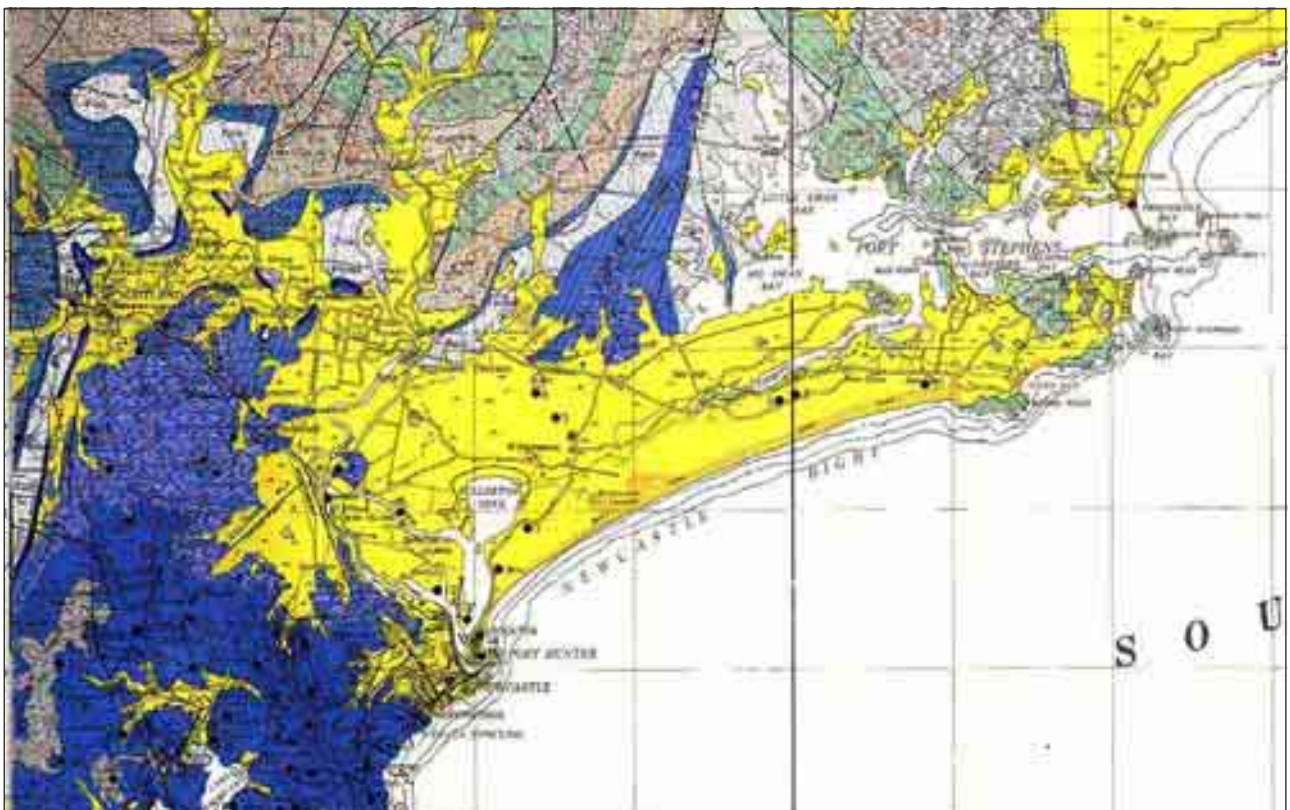
City Character covers those elements that collectively determine the public domain such as the way buildings address the street, the design of the building envelope, heritage conservation, streetscape and open space design, placemaking and quality of pedestrianised spaces. The relationship between these elements requires careful consideration to achieve the aim of providing 'people with a high amenity environment in which to live, work and shop'.

Social Planning addresses those issues that pertain to creating an equitable and sustainable community including accessibility, affordable social and housing mix, community infrastructure and social impact assessment.

Environmental Design aims to support ESD principles through integration with and embellishment of natural features and values that support biodiversity within a local context; sustainable, compact and efficient building design; efficient use of natural resources including water conservation and waste management.



Typical geological transect through cliff section on Shortland Esplanade
Source: Field Geology of New South Wales.



Geological units of the Newcastle Region
Source: 1:250,000 Geological Series Newcastle



2.1 ENVIRONMENTAL REVIEW

A review of the environmental conditions of the site was undertaken. Due to both its man-made nature, and “edge” location, many issues relevant to the future development of the site were identified. These ranged through geology with regard to being earthquake prone; fill soils and groundwater interactions; “edge” conditions of wind, wave and tidal climate; flooding; contamination, and the issue of the decaying but historically important Lee Wharf.

Geology

Quaternary Deposits

The Newcastle CBD and Honeysuckle Site lie on Quaternary deposits comprising gravel, sand, clay, and “Waterloo Rock” marine and freshwater deposits.

Newcastle Earthquakes

As a result of lying on these deposits, the Newcastle CBD suffered additional shaking to what would have occurred had it been founded upon rock.

Liquefaction

Liquefaction of saturated loose granular soils can occur during an earthquake, potentially reducing the load carrying capacity of structures. Studies on Lee Wharf suggested that liquefaction could occur to the loose sands underlying the Honeysuckle Site.

Soils

Fill

The top layer of soils on the site are generally 1.0m deep, comprising sand with varying amounts of rubble, gravel and other material such as slag.

Sand

From approx. 1.0m below ground level, the site soils are characterised by:

- some aeolian sand to 2.8m. (probably dredged material); underlain by
- ‘in-situ’ medium grained sand with shells to a minimum of 10m deep.

Rock

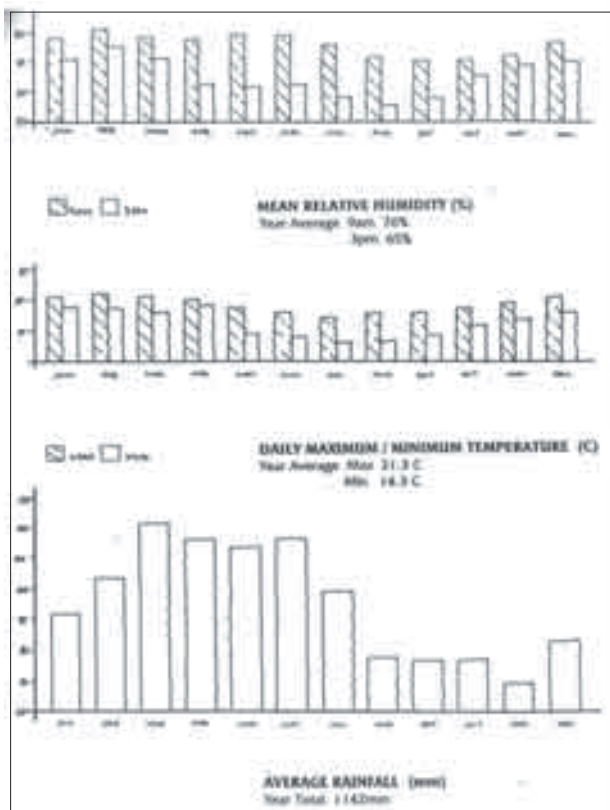
Beneath this sand layer, the site is founded upon interbedded siltstone and sandstones at a depth of 12m – 15m.

Acid sulphate soils

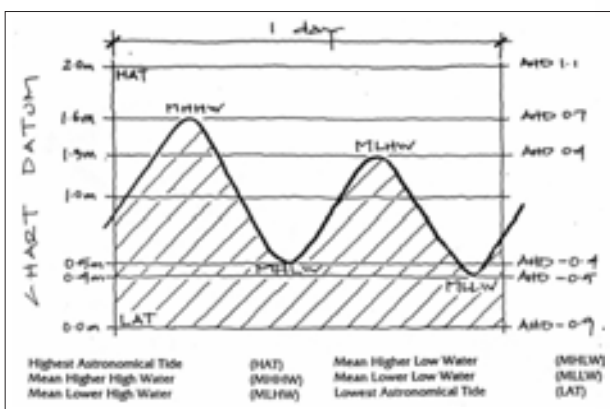
Acid sulphate soils are unlikely to occur on site.



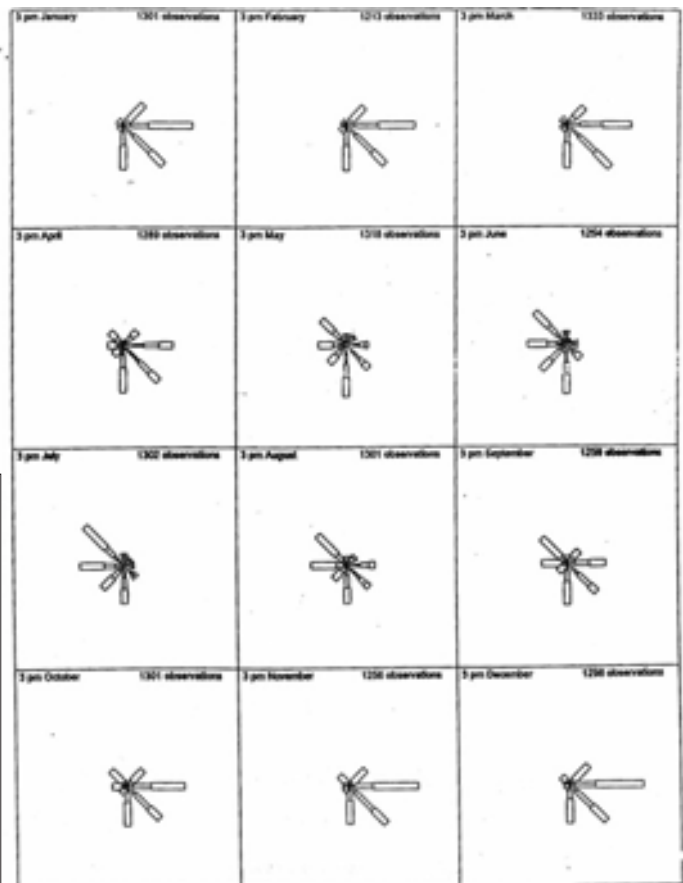
The geological profile



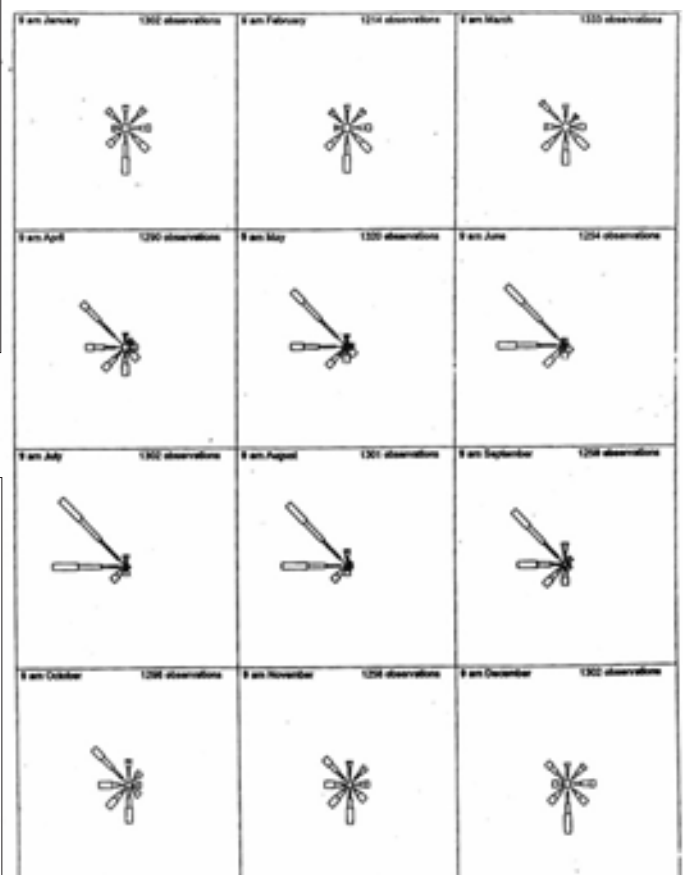
Humidity, temperature and rainfall graphs across a calendar year.



Tidal Chart



Wind diagrams - 9am



Wind diagrams - 3pm

Soil Amelioration for Tree Planting

Generally the strategy recommends that where feasible tree planting take place within insitu site soils. Prior to detail design development, soil sampling and testing by a NATA registered laboratory will need to be undertaken to determine appropriate site specific soil amelioration specifications. Further, the use of structural soils may be considered necessary for amenity planting within the public domain. Amenity planting design will need to respond to other environmental conditions such as salt spray, wind condition and a high water table which approximately corresponds with the rise of the tides.

Opportunities

Due to the sandy nature of the site, the opportunity exists to infiltrate stormwater within the site. Additionally, subject to groundwater quality testing, the opportunity exists to use groundwater for irrigation onsite.

Winds, Tides and Wave Climate

Winds

Predominately from the north-west, the prevailing winter winds have a significant impact on the conditions of comfort experienced on the site. Summer winds from the northeast are also a consideration and protection from both of these must be provided in the open spaces within Honeysuckle. Tunnel effects are to be avoided by appropriate orientation of built form, use of vegetative wind buffers and articulated facades.

Tidal Planes

The site has two high tides and two low tides daily, with significant inequality in range between successive high and low tides.

The maximum tidal velocity on the flood tide is 0.94m/s.
The maximum tidal velocity on the ebb tide is 0.99m/s.

Local Wind Generated Waves

The most significant wind waves are generated from the north-west. Site design will need to provide a:

- Weekly max./min. wave climate design level of 0.4m at a 2.0 - 2.5 sec. wave period; and,
- 1 in 50 year estimated wave height of 0.57m at a 1.9 sec. wave period.

Boat Wake

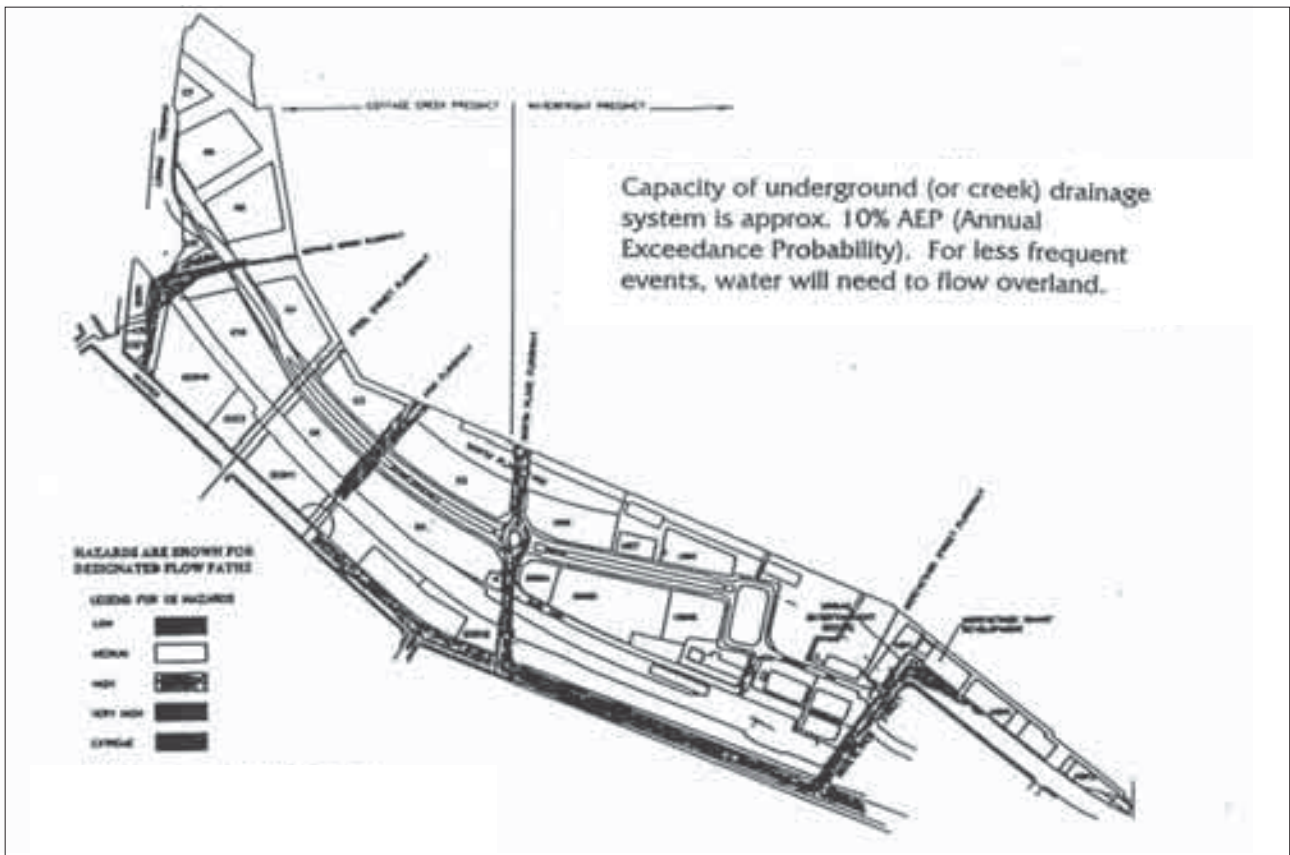
On average, 80- 100 boats pass Honeysuckle daily. The maximum boat wave (tugboat 200m offshore) has been measured at 0.26m.

Opportunities

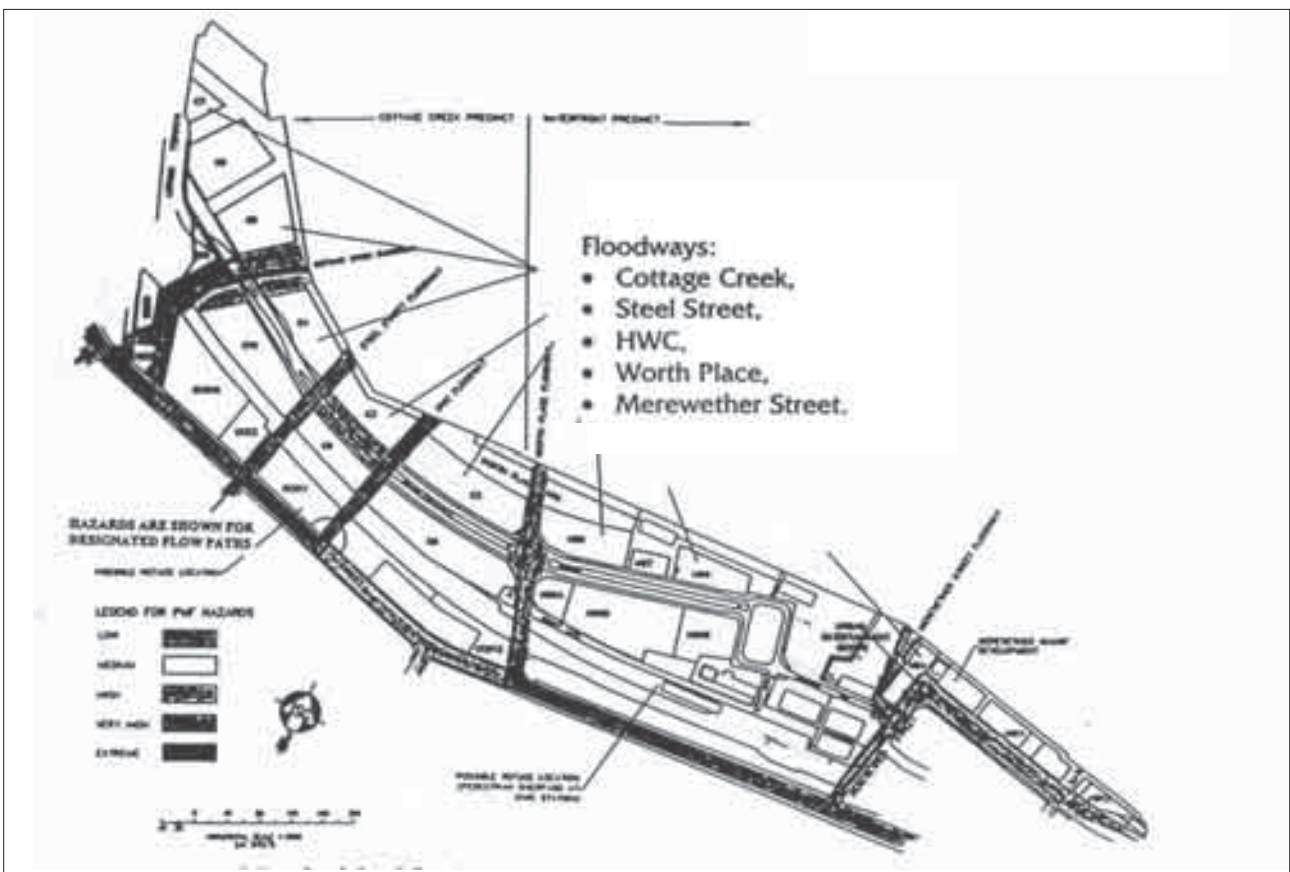
Significant opportunities exist to harness and express these natural processes in the form of site specific urban art installations. Examples include wind sculptures, acoustic listening stations, tidal movement systems, wind and solar collection and power generation.



Windmill at Kooragong



Extent of 1% AEP Hazard
Source: Waterfront and Cottage Creek Flood Management Plan



Extent of Probable Maximum Flood (PMF) Hazard
Source: Waterfront and Cottage Creek Flood Management Plan

Flooding

The capacity of the Cottage Creek drainage system is approximately equal to the 10% AEP (Annual Exceedance Probability). For less frequent events, water will need to flow overland within designated floodways.

Proposed Floodways include:

- Cottage Creek;
- the Steel Street view corridor;
- the HWC view corridor;
- Worth Place; and,
- Merewether Street.

Reference: For further details regarding basement parking requirements, proposed building pad levels and general floodway management guidelines, reference should be made to the *Waterfront and Cottage Creeks Flood Management Plan* HDC 1998.

Contamination

Soils

Some contamination of surface soils between 0.5m and 1.0m depth with heavy metals (copper, lead zinc and arsenic) and PAHs has been identified. However this has been characterised as low level contamination. At some areas within the site, the following guidelines have been exceeded:

- ANZECC B (further investigation threshold);
- EPA Provisional Phytotoxicity-based investigation levels; and in some cases,
- Relevant health based investigation guidelines.

Reference: For further details regarding soil contamination reference should be made to *Site Audit Statements and Site Audit Reports* HDC 1998.

However, importantly, the site soils show low potential for leaching of contaminants.

Studies indicate remediation of site soil for heavy metals is not considered necessary.

Groundwater

Some heavy metals are present in the groundwater above ANZECC guidelines for the Protection of Marine Ecosystems. However, these are typical of background levels in the Newcastle area and are expected to have no significant impact on the harbour.

Generally studies indicate that remediation of groundwater for heavy metals is not necessary, however, site specific checks may be required.

Harbour

The harbour water quality is similar to groundwater. Studies indicate the harbour water quality is considered acceptable and capable of supporting a diversity of aquatic life.

Stormwater

The stormwater contaminant load is far greater than that present within the groundwater. The management of stormwater runoff both through and from the site is fundamental.

Opportunities for pro-active stormwater management practices exist in locations such as detention and filtration through constructed wetlands and macrophyte soakers exist within Cottage Creek, along the western section of Honeysuckle Drive and within Worth Place Park.

Lee Wharf



The other side - under the wharf

Reference: Future design development of the public domain associated with the foreshore edge and Lee Wharf structure should make reference to [Honeysuckle Redevelopment: Remedial Work to Lee Wharf No. 1, 2 and 3: Concept Design Report, Issue No. 3](#) DPWS 1996

Seawall

Approximately 50% of the existing Lee Wharf seawall needs replacing. An opportunity exists with the proposed seawall rectification requirements to introduce alternative foreshore edge treatments that create a variety of foreshore experiences and improve access to the water. In particular the proposed foreshore space associated with Worth Place Park could include access down to the water's edge - a unique experience along the Newcastle Foreshore.

Wharf

The wharf comprises 1,100 timber piles, of which marine borer attack will lead to the eventual failure of most within the tidal zone.

A heritage study undertaken for the wharf noted that ... "To retain the significance of the 1910 warehouses it is important that the wharf structure remains in its existing or similar format".

The Strategy recognises the unique heritage significance of the wharf structure as an industrial artefact of the Newcastle foreshore time line. The decaying condition of the structure should be celebrated and integrated (visually) into the Honeysuckle public domain, whilst ensuring public safety.

The previous proposal to partially reconstruct the wharf has now been abandoned. However, there still remain opportunities for limited reconstruction in appropriate areas such as around the Lee Wharf buildings to facilitate public access associated with their adaptive reuse.

The Strategy recommends that the visual integrity of the existing wharf structure be maintained when viewed from vantage points such as from the Tree of Knowledge and the harbour. The Strategy also recognises the inherent public safety issues associated with retention of a decaying wharf structure. It is recommended that partial demolition of the wharf adjacent to the waterfront promenade be undertaken to prevent public access, and that limited intervention works be undertaken to the remaining structure to ensure the short-medium term stability of the structure.



Detail of the existing wharf structure

Survey by Lieut. C. Jeffries, Part of Hunter's sketch of Coal River, March 1816 Redrawn by D. Bairstow from a copy of the original in P.R.O London (as reproduced in Turner 1982)



1929 aerial view (Milton Kent Collection, Newcastle Region Public Library)

Harbour Edge

Prior to construction of the seawalls and long-shore wharves, the edge between land and water remained in a state of flux. Awabakal place names are telling of the pre-reclamation character of the various river mouth environments: tidal mudflats, saline swamps, tidal creeks and eucalypt sclerophyll forest; offering a diversity of flora, fauna and fishing.

Newcastle's harbour is said to be its greatest disadvantage, altered constantly by dredging, breakwaters and shoring, as well as the natural shifting of the estuarine deposits (Docherty 1983, p.2). Wreckages appear on early maps, marking the hazard posed to shipping at the harbour entrance. 'Tracks' or ship routes manoeuvring through shallow waters, and associated network of stone perches, mooring posts, warping buoys and beacons were also marked. Shifting aeolian sand to the east and alluvial mud and silt deposited from the west appear variously as banks, islands, estuaries, swamps and 'partly' dry fringes.

Later maps show how the extent of reclamation altered these shifting mudflats and channels to create dryland and deep water. The new edge absorbed Honeysuckle Point, filled the bay at the mouth of Throsby Creek and caused the demolition of the Carrington (Bullock Island) Bridge to allow for extension of the railway lands.

As revealed by overlay plans, what was sea is now land and what was land is now sea.

•
•
•
•
Muloobinba (Newcastle)
Place of the sea fern
•
•
Onebygamba (Carrington)
Mud crab place
•
•
•
Awabakal
People of the flat place
•
•
Meekarlbah (Civic)
Plenty honey here



Fishing near the Dolphin Pilings,
now No5 Lee Wharf.
Source: Newcastle Region Public Library

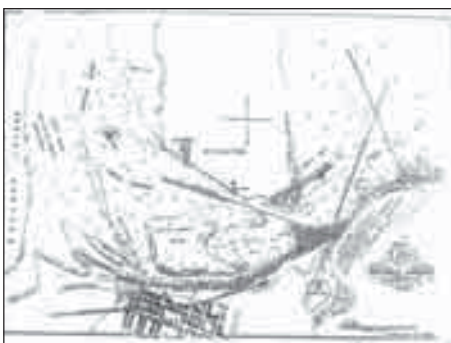
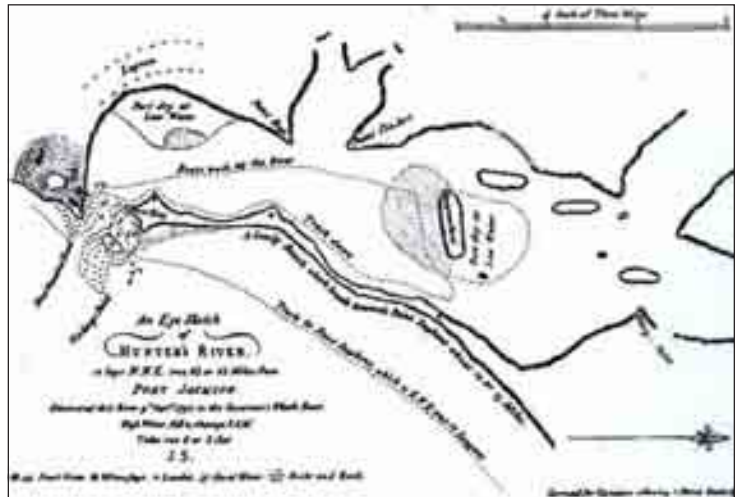


Chart of Newcastle Harbour 1867
Source: Mitchell Library



Maritime Services Board photograph of new No5. Lee Wharf where previously stood the Wickham Dolphins 1960. Source: Mitchell Library Small Picture File



An Eye Sketch of The Hunter River, 1797 in which Cottage Creek was mistaken as a lagoon. Source: Turner 1997 from the Turner Collection

The hill at the back of the gas works was then covered with honeysuckle, and was used by the Aboriginal tribes as a camping ground. In that year, "King Bully," the last of the kings of the Newcastle tribes of Aboriginals, died, and was buried close to the camp.

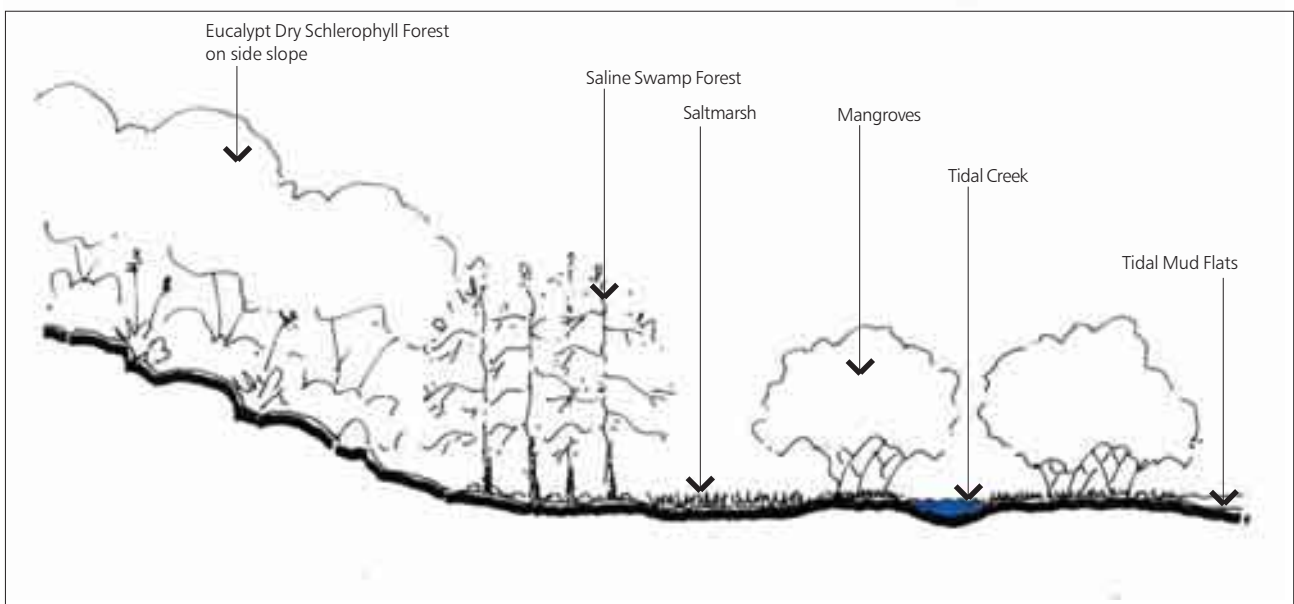
Windross & Ralston 1897, p.32

the quantity of oyster shells on the beaches inland is beyond conception: they are in some places for miles. These are four feet deep, without either sand or earth.

Vessels might lay within a few yards of where they are found

Paterson writing to Governor King in 1801

(Turner 1980, p.18)



Typical section through estuarine ecotone



Land systems of the Hunter Region

There is a closeness to nature- the river and the sea- the feeling of rootedness in a 'known' natural environment which is perhaps the essence of the 'genius loci', but to understand Newcastle we have to understand the port and its ancillary industrial installations.
Peter Proudfoot 1997

Tidal Mudflats

The estuarine qualities of the pre-settlement mouth of the Hunter are revealed in oral histories of the Aboriginal people and in the early diaries and sketches of explorers and settlers. The alluvial, tidal and wave forces of the delta continue to challenge the city's pattern of development of the harbour edge. Massive deposits of shells were remarked upon. There is some debate about the cause of these deposits as either the fluctuations of the river or Aboriginal middens. Limeburning on the foreshores of the lower Hunter River by 1821 yielded 172 000 bushels of lime, exported for use in construction, a significant contribution to Macquarie's Sydney (Turner 1980, p.18)

Vegetation community-

Monoculture of mangrove

- Dense Grey mangrove *Avicennia marina*
- River mangrove *Aegiceras corniculatum*

Tidal Creeks

Mistaken for a lagoon by Lieut. Shortland on his first visit, Throsby Creek and Cottage Creeks were part of a constantly shifting alluvial delta. The changing nature of the estuary can be traced through a comparison of Shortland's sketch c.1798 with a survey by Lieut. C. Jeffries 1816 and the Chart of the Newcastle Harbour 1867.

Vegetation community-

Narrow strips of mangrove

- River mangrove *Aegiceras corniculatum*

Saline Swamps

A patchwork of distinct vegetation communities characterised the estuarine swamps. Subtle variations in tidal conditions, such as water level and salinity determined their distribution.

Vegetation communities-

Saltmarsh: patches of occasional inundation

- Sea rush *Juncus kraussii*
- Coastal club-rush *Bolboscoenus caldwellii*
- Glasswort *Sarcocornia quinqueflora*

Pure stands of salt tolerant trees

- Swamp oak *Casuarina glauca*
- Paperbark *Melaleuca quinquenervia*

Scattered trees

- Cabbage tree palm *Livistona australis*

Eucalypt Dry Sclerophyll Forest

On higher ground the diverse woodland flora and fauna provided an abundant source of food for the Awabakal. Stories of plentiful kangaroo attest to this.

Vegetation community-

Wooded slopes rich in diversity

- Red bloodwood *Eucalyptus gummifera*
- Spotted gum *E. maculata*
- Swamp mahogany *E. robusta*
- Old man banksia, also known as 'Crooked honeysuckle' *Banksia serrata*
- Forest oak *Casuarina torulosa*
- Gynea lily *Doryanthes excelsa*



Coal staiths of the Australian Agricultural Company, 1933



Fleet of ships at Newcastle, B. W. Champion c.1912
Sources: Newcastle Regional Library



Stone cutting and storage site near A.A.Company (now Fannys) office in 1902.
Source: Snowball Collection



Plan of Rail Lines through Church Estate c.1857.
Newcastle Region Public Library. Source: Suters Architects Snell 1990



Newcastle Harbour Improvements Plan 1916 Site Plan.
Source: (Doring 1990)

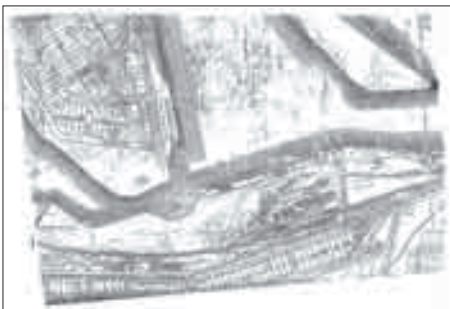
It does not appear that there is any other thing requisite in the formation of bay salt than to evaporate the seawater with an exceeding gentle heat; and it is even very probable that our common sea salt by a second solution and crystallisation, might attain the requisite degree of purity.

Sydney Gazette c.1804

Industry and Shipping



Newcastle Waterfront 1947-Hardwood logs loading for export overseas.
Photograph by M.Dupain.



Railways at Honeysuckle Point 1884.
Source: Turner 1994

Since long before European settlement the mouth of the Hunter River has served as a place of human industry. Toolmaking in particular has been an evolutionary activity in the Honeysuckle locality, through Aboriginal occupation, colonial enterprise and the development of mining, shipping and rail. Honeysuckle Point has been the location of fishing, foundries supplying shipping and mining equipment, shipbuilding, loco workshops and general goods handling. Other industries carried out on the site include timber export, saltmaking and limeburning.

Timbergetting and Export

1797 –1819: Rafts of timber were floated downstream to the timberyards established on the Honeysuckle waterfront. The following summary of timber use is derived from Turner, *Manufacturing in Newcastle 1801-1900* (1980, pp14-15).

Cedar *Cedrela toona* var. *australis*

- the most sought after hinterland timber, used for house fittings

Brush Cypress *Callitris macleayana*

- floorings

Beefwood *Stenocarpus salignis*

- shingles

Rosewood *Dysoxylon fraserianum*

- suited to turning, so used for fine furniture

Gum *Eucalyptus saligna*, *Eu grandis*

- roof timbers, carts, shipping, agricultural equipment

Mangroves *Avicennia officinalis*

- manufacture of felloes and stocks of wheels for the frameworks of ships

Tuckeroo *Cupaniopsis anacardioides*

- potential as a dye, though protected by Macquarie

Saltmaking

The primary form of manufacturing until 1841, saltmaking initially used solar evaporation, in ponds near the waters edge. The salt was used chiefly to preserve meat and salt sealskins and soon took advantage of the ready supply of coal at the waterfront. A pan imported from England in 1804 was erected, however there are few records of the technique used.

Limeburning

1808-1821: Middens and live oyster shells were abundant and used for limeburning. Shells from Fullerton Cove were brought to brick kilns on Honeysuckle Point. On islands where mangroves were available as a source of fuel, burning was carried out in 'frames'.

Process & Transport of Agricultural Produce

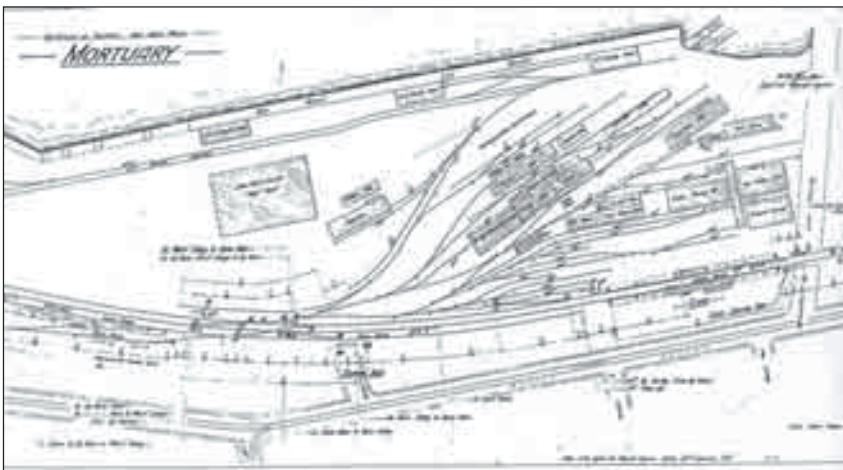
Meat canning, wool handling and tweed manufacture were active on the waterfront during the 1840's. In 1869, three soap and candle factories were located close to the Wickham slaughter yards to take advantage of the tallow flowing from the 5,000 cattle and 50,000 sheep.



Coal loading c. 1914. Source: Turner 1997



Lee Wharf, 1950.
Source: Newcastle Region Public Library



Mortuary Plan, 1937 SRA Archives.
Source: Suters Architects Snell 1990

It is greatly to the credit of the successive Loco Branch Workshops supervisors at Honeysuckle Point that the Loco Workshops managed to turn out the enormous quantities of work that were handled over the 55 years they were in full operation, despite the many inconveniences in the layout of the shops to which the workers were subjected.

Doring 1990, p.22



Lee Wharf storage sheds- undated.
Source: Newcastle Region Public Library

As the largest single establishment of the Great Northern Railway, and a vital contributor to its former operation as the land transporter for the Region's major coal export industry, the Workshops Group reflects the importance of, and intimate association between, railway and ocean transport in the late 19th and early 20th Century.

Doring 1990, p.48



Lee Wharf, 1953.
Source: Newcastle Region Public Library

Manyama families are compelled to live in the immediate vicinity of the oil tanks, as they are unable to habitation in other parts of the district. Those people have been living in constant dread of disaster for a number of years. One valuable life was lost, and perhaps only providence prevented greater loss.

W.B.Harvey (NMH 21/12/39)

Spectacle of a Busy Port and Railway

The mass and bulk of goods awaiting shipment and the activity of the wharves and rolling stock of the loco workshops are fundamental to the character of Honeysuckle over time. The Honeysuckle Point Heritage Study has established the significance of traces that remain of railway and wharf activity (Doring 1990).

Loco Manufacturing

Opened in 1856, the Loco Workshops on Honeysuckle Point handled all the needs of the rolling stock. The foundry supplied windlasses, winches, iron blocks, cranes, crabs, axles and wheels for wagons, hay presses and gears for mills and machines. Though layout was vital for efficient workflow of rolling stock, the Loco Workshops were laid out wherever space permitted, as determined by the more orderly Per Way Workshops. The Loco Workshops were expanded in c.1885 and in 1920 the new erecting shop was offset to the east from the old one to allow more space for the traverser, cranes and around the engines.

Per Way Workshops

This section of the Honeysuckle railway lands was responsible for design and construction of new lines and railway buildings from 1878. The Per Way Branch was capable of a diverse range of manufacturing and materials fabrication and until the Depression. The land upon which the Railway buildings and lines stood contains evidence of 'their former layout and equipment, in the form of footings, chimney bases and flues, inspection pits, pipelines etc which will enable interpretation of the original layout of the workshops and machinery' (Doring 1990, p.51). Similarly, the traverser was a fundamental part of the Workshop operations and any relic of its presence would assist interpretation.

Lee Wharf, Cargo Sheds and Railway Tracks

The striking appearance of the cargo sheds, in contrast with the 19th century railway buildings, offer a highly visible and meaningful feature of the public domain. These structures signify the shift from coal dominated export to general cargo. To migrants, they represent the point of arrival. Other significant remaining features of the working industrial harbour environment such as railway tracks, loading stages and the wharf itself can become important interpretive elements on the site as part of the context for future development.

Expansion

In the 1920's and 1930's the location of oil storage at Throsby No. 1 became controversial. The threat to human life caused by early mining efforts, goods handling and heavy storms was replaced by the risk of explosion as the wharfs expanded to cater for larger scale shipping and vehicular transport. Human movement became dwarfed and threatened by these changes to Throsby Wharf in the 1960's, leading to a call from urban design critics for improved interaction at the waterfront as part of the future development of Honeysuckle.



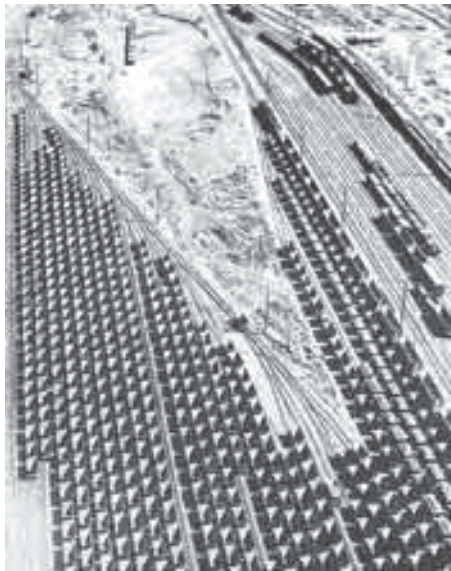
1960s scene.
Source: Lee Wharf file
Newcastle Region Public Library

The coal shaft is sunk upon the summit of the hill, and the coals carted down by bullocks; but from the defensive nature of the working, and the lazy habits of the incorrigibles who are sentenced to this labour, the produce does not at all correspond with what may be expected when a more efficient system is introduced.

Naval Surgeon Cunningham



Drawing of Newcastle, 1874 Illustrated Sydney News Source: Turner 1997



Nest of 8-ton coal wagons, superceded by the larger trucks visible on the right, c.1967.
Source: Barney & Callen 1989

The mining of the coal has created a labyrinthine underworld, ramified and interconnected, under the entire city and in general this honeycomb, although invisible, brings us a sense of an autochthonous structure, governed by the ancient forces of the earth; we are brought 'down' and given roots.

Peter Proudfoot 1997

Mining proved both costly and dangerous, and although the authorities did not pay much attention to the death or injuries of the prisoners in the workings, a strong feeling prevailed towards abandoning coal mining by convict labour.

Windross & Ralston 1897

Essentially a seaport and a coally seaport. Every third house sells slops or ropes or blocks or some of the many other articles required by those who go down to the sea in ships. Whilst if you pass three persons talking together you will be sure to hear that their conversation is about coals

SMH 20 Aug 1866

Coal and the 'genius loci' of Newcastle

Over sixty million tons of coal were raised in the Hunter district in the 19th century. Newcastle was known as a 'coalopolis' from 1822-1847. The earliest efforts in coal mining at Newcastle were concentrated around the southern shore, and the role of both early mines and coal export in shaping the city and sense of place was fundamental to its development history.

Coal staiths and shoots

1831: Australian Agricultural Company, having attained the coal mining rights from the Government, constructs the first of a growing network of structures overlaying the city grid in sweeping arcs to expedite the movement of coal. Private coal loading shoots or staiths and a proposed government wharf were shown on a [Plan of Extension into Newcastle](#), 1857, to the east of the 'Church Estate' of Honeysuckle Point.



Coal facilities

Wagons

The 8-ton coal wagon, superseded by the current type and relocated, was once ubiquitous on the coal staiths and then railway tracks of Newcastle's waterfront, forming a network of branching lines and repetitive forms.

Transfer of Coal Industry

All coal traffic was relocated to the Carrington Basin or Bullock Island coal loading facilities during the 1880's leaving valuable space at the Honeysuckle Workshops for loco works, general goods and passenger traffic. The emphasis on human handling and diverse cargo is unique to the Lee Wharf.

Travelling Hydraulic Cranes

The Pumping Engine and the two pumps at Bullock Island powered the Honeysuckle Workshops as well as the coal loading cranes on Carrington Dyke. Fabricated in the Per Way Workshops at Honeysuckle Point, these hydraulic cranes dominated the harbour view.



Coal trucks arriving at the Port, Snowball Collection
Source: Barney & Callen 1989



Wooden bridge constructed in 1831 and new iron bridge built in 1864 linked 'A' Pit to the Australian Agricultural Company wharf.
Source: Mitchell Library

Carrington is a peninsula jutting out into Newcastle harbour, at the heart of the city and in the midst of its industrial core. The major access route is a bridge from the wharf-side, that leads only to Carrington. The bridge is a symbolic entry and exit point to and from the suburb. Local people regularly refer to 'outsiders' and speak of the peninsula suburb as an 'island' with strongly identified boundaries between their place and the rest of the city.

**Hilary P. M. Winchester, Kevin M. Dunn,
Pauline M. McGuirk. 1997**



The coal arch built for the centenary of Newcastle in September 1897.
Source: Turner 1997

As far as the eye could reach Hunter Street was ablaze with illuminations of various design and colour, and the coloured fire arising from the procession and from the top of the arch spanning the street at the intersection of Perkins Street added grandeur to the scene. Hunter street after the procession had passed became one surging mass of humanity.

Newcastle Morning Herald, 1897

Connections

Implied connections to other places such as Lake Macquarie were very much a part of Awabakal life in Muloobinba (Newcastle), just as the port was economically linked to European settlements in the upper catchment. Since the earliest European inhabitancy of Newcastle, physical and symbolic connections have been expressed in the landscape of city.

Breakwater

Construction was carried out between 1818 and 1846 to join Nobby's to the shore. Successive breakwaters have been since added, constructed from ballast and wreckage.

Coal bridge

A wooden bridge over Hunter Street linked the Australian Agricultural company 'A' pit to the wharf in 1831. This connection was expanded in 1864 with the construction of the iron bridge.

Rail connection

By 1857 railway connected Honeysuckle Point and Maitland, resulting in the need for pedestrian overpasses, such as the ornate iron bridge at Honeysuckle Station.

Coal Arch

A ceremonial arch was erected for the centenary in September 1897. Arches, awnings and bridges were decorated with greenery and illuminated for the occasion, demonstrating great civic pride.

Bullock Island Bridge

Bullock Island Bridge, Darvall Street Bridge and Cowper Street Bridge linked Carrington c.1900 and were demolished as demand for longshore wharfage and dredging prevailed over the connection. Bullock Island Bridge (also known as Carrington Island or Denison Street Bridge) connected Carrington and the western shore of Honeysuckle Point at Worth Place, with a wooden bridge and causeway construction. A gravity fed water pipeline supplied Honeysuckle from Bullock Island and it is possible that a high pressure pipeline crossed the Bullock Island Bridge prior to 1878 (Doring 1990, p.16).

Bridges over the Hunter, Bellinger and Hawkesbury

The Honeysuckle Per Way Workshops later fabricated materials for a number of bridges including Cottage Creek, Throsby Basin; Ironbark Creek, Hexham; Hunter River, Singleton; Glennies Creek, Singleton; Warialda; Peel River, Attunga; Hunter River, Aberdeen; Styx Creek, Newcastle; Bellinger River, Macksville, and Sydney Harbour Bridge.

Horse Punt to Vehicular Ferry

Long distance traffic along the coast and local traffic between Stockton and Newcastle was linked by the steam vessel Mildred and later, the oil fuelled Kooroongaba.



Bullock Island Bridge. Snowball Collection
Source: Barney & Callen 1989



Honeysuckle Station. Source Mitchell Library



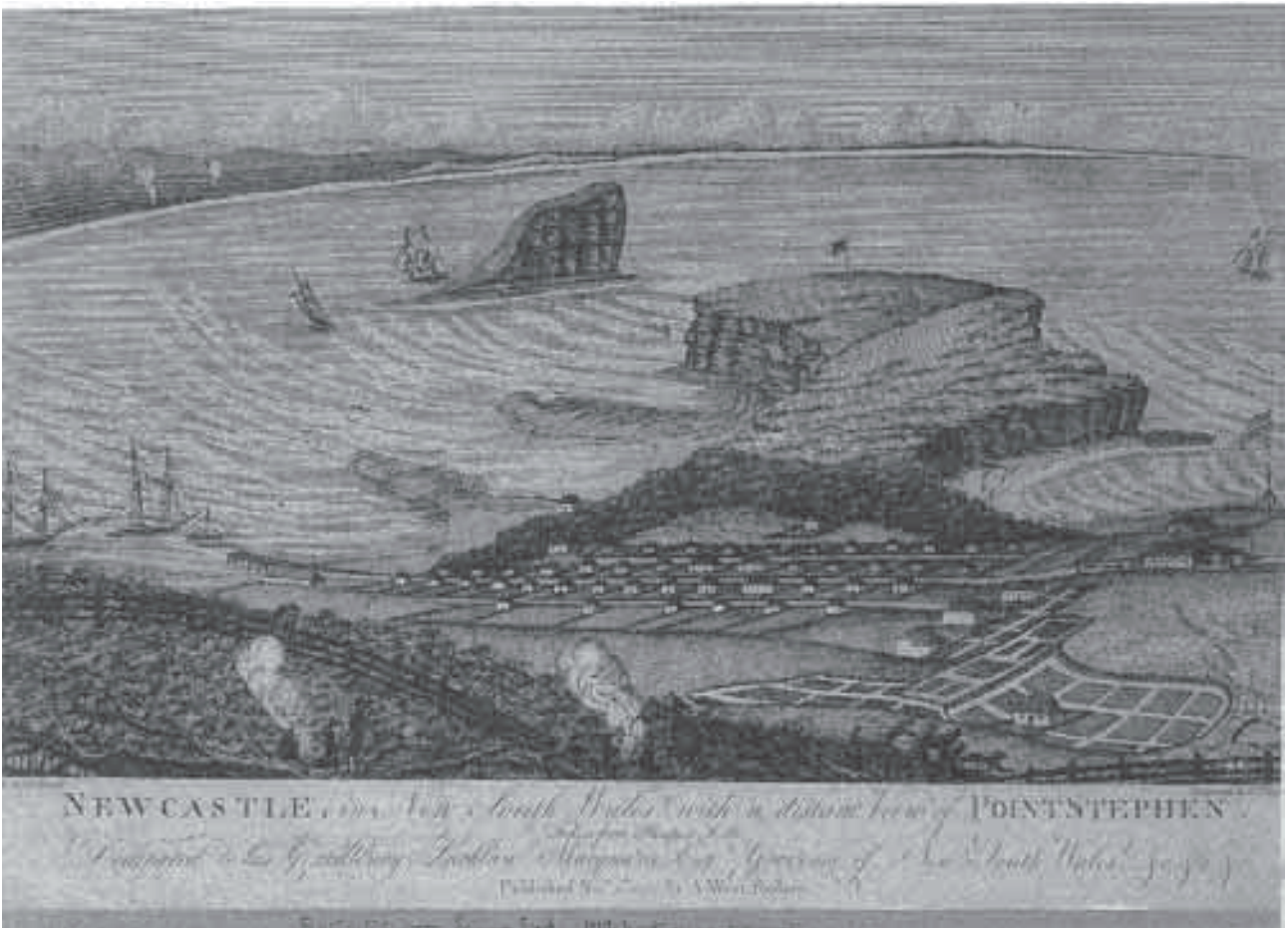
The wreck of the Adolphe aground on the Oyster Bank, 1904. Source: Barney & Callen



Carriage Shed solar power collectors.
Source: Illumination Newcastle

... the transfiguring event for Newcastle was a natural disaster. The impact of the earthquake at 10.27am on Thursday, 28th December, 1989, was officially 5.5 on the richter Scale. It shook the city to its very foundations, killing twelve and injuring one hundred and thirty-six people, destroying several hundred commercial buildings and leading to a record payout of more than \$860 million in insurance claims. It struck at the commercial heart of the city and at one of Newcastle's finest inner city areas, Hamilton. Its impact was more than physical, however, for it exposed to the populace at large the city's political fractures and the seams of hierarchical power amongst the elites of the various contending stakeholders in the city. Unplanned, the earthquake forced Novocastrians to rethink their city and themselves in the midst of their own disrupted social and political order. It also forced them to plan at once for the longer term before the exposed political topography covered itself over again and hardened into embedded power blocks where "pre-disaster aminosities (could) re-emerge and (be) compounded by new issues relating to reconstruction"

Trevor Hogan 1997



Newcastle in New South Wales with a distant view of Point Stephens, T.R. Brown, Collection of Newcastle Region Art Gallery.
Source: Mitchell Library

Natural Energy

The forces and harnessing of natural energies have had a remarkable impact on human life in Newcastle. Learning to harness these forms of energy has long been a Novacastrian tradition and is finding new meaning as Newcastle turns to alternative energy forms as in the Illumination Newcastle project.

Windmill

The windmill on 'The Hill' acted as a navigational landmark and was later replaced by the obelisk. Today the Kooragang Island Wind Turbine dominates the horizon on approach to the city, an example of public-private partnership for renewable energy development in the Hunter.

Evaporation

Initially, solar power was used to make salt from ponds of water on the shore. Now solar power is collected on the roof of No. 1 Carriage Shed at Foreshore Park, with the capacity to generate 6.5kW of electricity, contributing to a reduction of greenhouse gas emissions of 8.5 tonnes per year (Newcastle City Council)

Shifting Sand

Shifting sand was as much a phenomenon on land as in the estuary. Sand shifted after clearing of vegetation in 1843 causing collapse of barrack walls. In 1887, the Sand Drift Reclamation Act was created to tackle the effects of sand movement.

Wrecks

The impact of the elements was often remarkable, such as the storm of July 19, 1866 when five vessels 'Cawarra', 'William Watson', 'Seagull', 'Mary Rose' and 'Arthur' were wrecked and 60 lives were lost (Windross & Ralston 1897, p.26). Today plans are being prepared for the conversion of wave energy at the base of Nobby's headland into electricity.

Seismic activity

Earthquakes occurred in 1868, 1907, 1925 and 1989. The latter reached 5.5 on the Richter Scale and left an indelible mark on the Novacastrian psyche.

WRECKS- 1816- 1897

- 1870- March 20th, ketch 'Ino', on the oyster bank
- 1871- October 3rd, barque 'Catherine'
- 1876- July 21st ketch 'Kate' in collision with the steamer 'Morpeth' foundered
- 1877- July 15th, the steamer 'Yarra Yarra' foundered
- 1878- September 12th, the 'City of Newcastle' wrecked below Shepherds Hill
- 1879- May 3rd, schooner 'Margaret Chessel'
- 1897- June 19th the schooner 'Lost Peacock'
- 1880- August 8th steamers 'Canton' and 'Morpeth' collision;
- 1880- September 23rd the schooner 'Alert' wrecked
- 1882- August 13th, ketch 'Johnathan' and schooner 'May Newton' both wrecked
- 1883- May 9th, schooner 'Sarsfield'
- 1884- April 29th ketch 'Ada' wrecked
- 1884- July 3rd- ship 'Susan Gilmore' ashore at Shepherds Hill.
- 1886- October 4th Ketch 'Mary Ann' Ashore at the southern breakwater.
- 1888- June 5th ship 'Berbice' ashore at Stockton Beach.
- 1891- June 24th, schooner 'Ranger' wrecked on the Northern Breakwater.
- 1891- October 4th ketch 'Johnathan'
- 1893- March 9th, steamer 'Federal' ashore at Northern Breakwater.
- 1894- September 9th steamer 'Colonist' wrecked on the oyster bank
- 1895- December 22nd, barque 'Durisdeer' at Stockton Beach'.
- 1896- May 24th, steamer 'Byron' foundered off Redhead
- 1897- April 24th, barque 'Adderley' at Stockton Beach

Windross & Ralston 1897

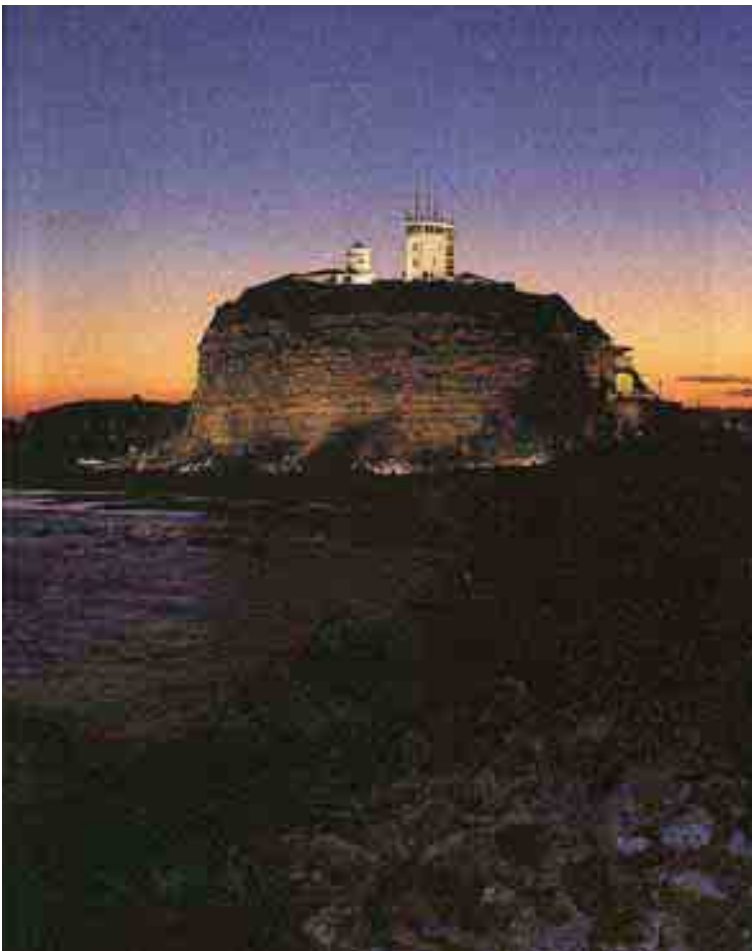


"Shipping at Sea".

Source: Barney & Callen 1989



Newcastle at night.
Photography by Allan Chawner.
Source: Illumination Newcastle



Nobbys Headland.
Photography by Allan Chawner.
Source: Illumination Newcastle

There are two of Tangye Bros. special steam pumps at this place, and in first class working order. I may state for your information that the works at Honeysuckle Point are supplied from this pumping station, also the hydraulic cranes for shipping coals at the dyke.

Locomotive Engineer. 1878, reporting on Pumping Engine and Pumps No.1 and No.2 at Bullock Island.

Doring 1990, p.16



Migrants luggage for Greta Camp at Lee Wharf,
Newcastle Morning Herald, 1950

Source: Newcastle Region Public Library

The Workshops site is associated with the initiation of statewide management of electrical power distribution and the earliest period in the history of the NSW Electricity Commission when the Boiler Shop (no.6) housed one of the first power stations to be set up and run by Elcom, from 1951-1957.

Doring 1990, p.48

Human Energy

The generation of power is central to many aspects of life in Newcastle. The mining of raw materials, and achievement in engineering, transportation and haulage are evident in the city's fabric. At Honeysuckle, the past experience of generated power is embodied in the remaining rail lines and workshops, subterranean mine workings, and Lee Wharf itself. The existence of the stone crushing works and ice making facilities associated with the port activities however, are no longer evident, yet the idea of power is active on the harbour and a fundamental part of the lived experience of the city.

Coal

Newcastle was the main supplier of fuel to the colonies. Honeysuckle was central to the workings of city's coalmines. The Merewether Wharf foreshore was dominated by the transfer of coal from the massive infrastructure of coal staithes and the inclined plane to shipping vessels.

Cargo handling

Later the handling of general cargo between rail and shipping occurred at Lee Wharf, as coal activity shifted to outer Newcastle and coal loading relocated to the northern foreshores of the harbour. The constant activity of cranes, ropes, wagons and trolleys, swiftly manoeuvred by the workers on Lee Wharf is another expression of high energy.

Steam

Steam dramatically changed sea and land transportation. In 1839 the Hunter River Navigation Company operated the first Australian steamships from Newcastle. Steam trams operated on the streets of Newcastle from 1887 until after the introduction of Electric trams in 1925 and were serviced by the Loco Workshops on Honeysuckle. Steam vapour still has a presence as a highly visual element of the port's industrial skyline.

Gas

City of Newcastle Gas and Coke Company was established in 1866 in close proximity to Cottage Creek. Streets and waterfront were gas-lit by 1875, changing the nature of the working day.

Electricity

Trams operating from 1923 until 1950.

Electric Lamp Manufacturers (Aust) was established in 1931.

Electrification of rail to Sydney was completed in 1984.

Explosives

Relegated to memory, the 1942 attack by a Japanese submarine 5.5 inch gun was mistaken for a military drill by some who went out into the street to enjoy the spectacle.



Energy biproducts
Photograph by Allan Chawner.
Source: Moore & Ostwald, 1997



Nest of coal wagons, c.1967.
Source: Barney & Callen 1989



Shipping coal at Newcastle,
Illustrated Sydney News, 23 March 1878.
Source: Turner 1982

*The most telling image of
Newcastle at war, however,
remains the night with its
brownout and grotesque
shadows, the movements of
restless human beings- many in
uniforms- and the silver penils of
light from the searchlights and
penerating "darkened arch of the
sky" and relentlessly roaming
across the clouds*
John Ramsland 1997



Carrington Pump House at night.
Detail of photograph by Allan Chawner
Source: Illumination Newcastle



'38 Class Pacific 3801 takes on water at Maitland at night.'
detail of postcard photograph by Andy Warren , 1998

Artificial Light

Artificial light is a recurrent motif through anecdotes of the past and the living experience of the city. Vividly described in literature and appearing in a multitude of ways, innovative use of artificial light is as relevant to the city today as it was essential during wartime. This is demonstrated by the Illumination Newcastle project, harnessing alternative energy to bring heritage elements to life in the awesome context of the modern industrial port. Those visible from Honeysuckle include: Nobby's headland, Dyke Point Dockyard, the James Barnet designed Carrington Pumphouse, the Carrington Bridge, the Tree of Knowledge and the Honeysuckle Workshops.

Navigation

The relationship between navigation and the built form of the city can be traced back to the relocation of the Wesley Mission which had obscured the navigation lights to ships.

Mine lanterns

Governor General Northcote held an underground banquet in A.A.Co. Sea Pit Mine, lit by Chinese Lanterns and the monogram of the King in fairylights.

Celebration

Procession and coal arch, built for the centenary, were 'ablaze' with illuminations of various designs and colour.

Blackouts

The Loco Boiler Shop was adapted to generate power from diesel in case of power failure, which were commonly caused by electricity shortages after WWII.

Brownouts

During WWII the city endured 'Brownout' where only the doors facing inland were allowed to be dimly illuminated, creating distorted shadows.

Searchlights

Searchlight installations were described by writer Dymphna Cusack in *Southern Steel*, 1953 as 'silver pencils of light'.

Furnaces

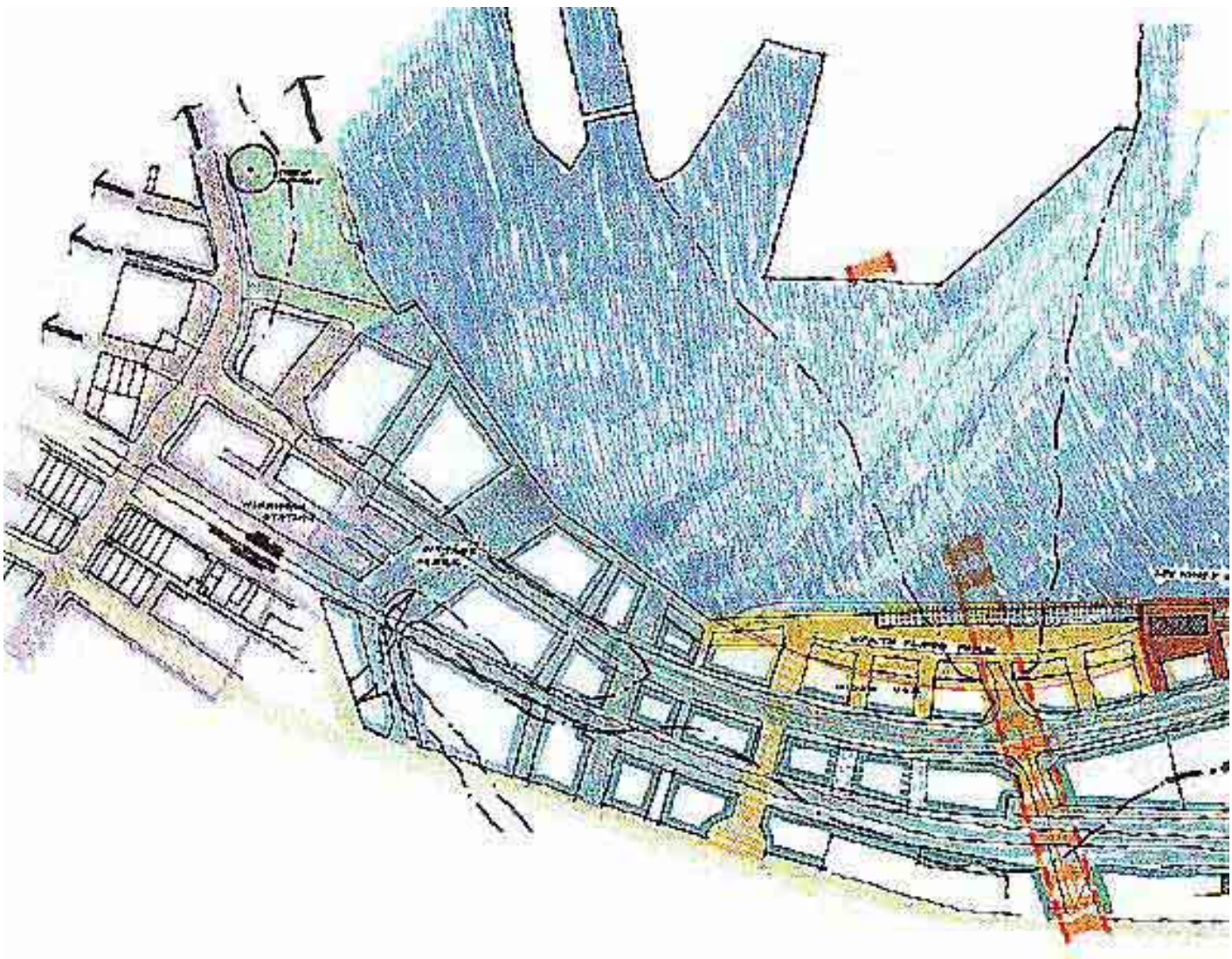
The BHP blast furnaces, glowed constantly, making an easy target for the enemy in WWII.

Arc lights

Strong floodlighting allowed work to continue into the night. Baths and wharves were lit by powerful arc lights.



The Coal Arch



The diagram used as a basis for the first round of consultation. Note the original water line shown dashed.

2.3 COMMUNITY CONSULTATION

If several events can be made to coincide and are allowed to stimulate each other the resulting activity is likely to be greater than the sum of activities originally involved. People come along and partake because something is already happening. A positive process has started.

Gehl 1980



The role and function of the Public Domain Strategy have been a core concern to the community and a variety of stakeholder groups including:

- Aboriginal community
- Youth
- Residents
- Academics
- Access
- Business
- General community
- Newcastle City Council

Stakeholder groups were consulted in a two phase process, the first being a series of meetings with focus groups and the open community, followed by a second round of presentations, dialogue and response in a more general meeting environment.

The input and response has been vital to the process, not in a determining sense, but rather at an informing level where ownership and self worth are the guiding factors. It is important to note that many of the comments recorded, whilst having a degree of validity, extend to issues beyond the scope of this strategy.

Comments were sorted into the following issues:

- Decision Making
- Access
- Amenity
- Site Usage
- Site Interpretation
- Design

These issues are expanded as follows.

Decision Making

Community consultation has highlighted the importance of including the broader community in the decision making process of the birthing of Honeysuckle. The people of Newcastle are genuinely excited about what is happening at Honeysuckle yet have become somewhat frustrated at both the lack of progress and lack of communication about process. It is important to establish an appropriate dialogue with the community, keep them informed and involved as much as possible. The consultation process of this strategy has gone some way to initiating this.

The community have identified the significance of this development in relationship to the overall city form and function and broader regional issues. In particular the second round of meetings almost became a platform for debating the future of the railway corridor and potential for light rail between Hamilton and Newcastle Stations. Honeysuckle Development Corporation is an important stakeholder in regard to this issue and could play an important advocacy role in shaping the future southern edge to the site.

Community Comments on Decision Making

Make sure the development feels like it is everyone's and that there is a sense of "ownership" in all the spaces.

Honeysuckle should use every opportunity to utilise or showcase local talent eg. opportunity near turnoff to Stockton - "wasted".

Do we have a picture of who is going to live there ?

Survival through change - Newcastle could be an alternative city through transport changes - social changes.

Newcastle people don't want to get public transport.

The face of Newcastle is changing - parking absorbs a lot of land so we need to consider alternative transport.

Road through heritage buildings is not working as a shared space for car users.

At a more detailed level, the development of the public domain should allow for participation of the local community in both ceremonial and creative opportunities. Ceremony has been an intrinsic part of the development of Newcastle and could be used to build public understanding, enthusiasm and ownership for the next wave of development in Newcastle. Examples include ceremonies to celebrate turning the first sod for each new development, opening of new developments (open day), tree planting, and festivals and gatherings on site to allow people to engage with the place, to build a memory of what it was like before redevelopment and to see it more than just a city bypass.

Likewise, community art opportunities using local artists engaged to facilitate creative outcomes within the public domain should be investigated. Once again, holding workshops on site, even if they only produce temporary outcomes/ installations, will serve to galvanize community ownership and allow people to tap into their underutilized creative energies. Examples include setting up temporary art studios in the heritage buildings for specific events/outcomes, developing temporary installations for Honeysuckle Drive until redevelopment occurs, establishing a community logbook or sketch journal where people can record thoughts, memories, concepts for Honeysuckle and greater Newcastle.

Access

The railway was viewed by many people as a barrier, both physically and mentally. Consequently, the intergration of possible connections between Honeysuckle and City West, City East and Civic was considered critical to the overall vitality of the city. The community is currently lobbying all levels of Government to consider light rail within the city corridor as a mechanism for integrating the city. Many believe the economic prosperity of the city is tied to the resolution of this issue. Although outside the scope of the strategy, it is acknowledged that this issue requires effective community action and government decision making so that a solution may be found in the long term.

What this document does is act as a guideline for moving forward, while keeping longer-term goals in sight. As with many issues raised by the Public Domain Strategy, the influx of population will, in time, make long term measures feasible. As well as the new activity on the waterfront, continuing industrial, community and natural environment access to the water was seen as highly desirable. There will be many opportunities for maintaining connections between Hunter Street and the Honeysuckle waterfront including The Palais on Hunter Street which is to become a multipurpose Youth Centre that will generate an influx of young people into the street network.





Existing carpark at Honeysuckle



Where to now... Mike Milligan, Jon Shinkfield and Annabel Stanton on site



Opportunity for a future infill building



Existing Civic Station, railway line and railway overbridge are the primary barriers between Civic and Honeysuckle

Community Comments on Access

On the other hand the provision for cyclists at the square-about and west of it is disliked by cyclists, who often prefer to ride along the asphalt of the Old Wharf Road. This is mostly caused by the location of trees in the cycleway.

Spill-over of skaters identified as a problem. Skating is now increasingly a means of transport and exists wherever there is flat paved land - Skateboard association needs to be formed to contribute to the strategy.

Crossings need to be acoustically clear. It is very difficult to get across Honeysuckle Road.

Worth Place could be a major access/entry point - then we can close off the eastern access (Hunter Street was designed as a commuter artery)

Breaking down the barriers to city is worth considering.

Vital to open up Honeysuckle to Hunter Street: all business people along Hunter Street felt that the railway has to go.

Connections between Honeysuckle and city west critical.

Did we include State Rail in our consultations? Rail corridor landscaping - city centre progress task force could be involved.

Ferry could be triangular route.

Need to consider railway line opportunities - railway environment could be more pleasant (greener)

What are the opportunities for crossing.

Pedestrian and wheeled pram/wheelchair access over railway-level crossings important.

Are there opportunities for tourist buses to arrive? Where do they stand? This will take up a large area.

Light rail is preferable to a bus system.

The core access issues can be summarised as follows:

Public Transport

Various forms of public transport were discussed.

- A focus on integration and coordination of public and private services was suggested to efficiently handle fluctuations in demand, particularly around special events.
- Ferry was strongly urged as a desirable mode of access, and may be warranted in the long term as demand increases.
- Commuter carparking adjacent to rail stations has been called for, suggesting the need for an integrated transit strategy for metropolitan Newcastle, which is beyond the scope of this Strategy.

Vehicular Circulation

- Parking was highlighted as a major issue, as development of vacant lots currently facilitating parking will be perceived as a loss of space.
- It was also agreed that there is the need for a shift in thinking to becoming an alternative city, with efficient and well used public transport system.
- The need for vehicle access provisions into the city and particularly for the access-challenged and for service vehicles was noted. Fears that shared space may not work, based on the current arrangement, were also heard.

Cyclist Circulation

Cycling was discussed at length, due to their specific requirements, summed up by the three Cs: Continuity, Connectivity and Convenience.

- The need to treat recreational and commuter cyclist needs individually and,
- separation between pedestrians and cyclists were seen as critical design considerations.
- Skateboarding, yet another mode of transport with its own requirements and characteristics, was to be considered in parallel with cyclist, pedestrians, road users and other wheeled access such as prams and wheelchairs.

Pedestrian Movement

- Road and rail crossing design was a key concern, with expressions of dissatisfaction about railway bridges and traffic islands.
- The idea of shared vehicle and pedestrian zones was generally well received, as it creates places where people dominate.

Amenity

The useability of the public domain depends upon a number of factors such as safety, orientation and adequate provision of services such as parking and seating.

- Need to address traffic flows and give pedestrian dominance in both the Urban Entertainment Precinct and the Cottage creek precinct
- Lighting, visibility and ease of casual surveillance were seen as things that make people feel safe.



Waterplay at The Entrance foreshore

the more people here the safer the place will be;
notion of emphasizing positives;
don't protest security issues;
security people should be interacting positively;
create a sense of ownership;
perception of fear is a reality;
safety by design - reduce/eliminate alcoves, fences -
flush at alleyways, lighting;
pavements should be maintained;
incorporate passive observation working within
public -space can maintain sense of safety;
2ft-6ft free for riding bikes and skateboards;
maintenance important: rundown - perceived as
unsafe;
graffiti - important to remove immediately. By the
third removal there is generally no re-occurrence;
graffiti-friendly places are not targeted;
skateboards - can be designed out - eg end of mall
- notches, gaps; and
prevent occurrences - rather than react
afterwards

Principles for Safety

Source: Tabled at Community Consultation



Open Air Cinema, Sydney

Given the fact that traffic movements are the most
frequent outdoor activities in most areas, efforts
should be made not only to integrate the various
modes of traffic, but also where possible to integrate
the traffic with other types of activities.

Gehl 1980.



Fishing at the docks

Community Comments on Amenity

Activity/cultural/community centre will decrease vandalism.

There is a middle ground between making a place secure and allowing people to 'hang'. All people have a right to be there. Safety related to aesthetic of a place. What is this space like at night.

How does the Public Domain Strategy provide for youth spending time in these places.

The idea of losing the working harbour is a major concern.

What people do respond to is the activity on the harbour - people are attracted to the water's edge.

- The specific needs for hearing, sight and mobility impaired were noted. Examples of these comments are; the danger to cane users of free-standing bulky elements on stem mountings, the need for leading edges and landings at road crossings and, the problems presented by vast areas of undifferentiated paving.
- Water should be incorporated as an attraction for people to play around, just as the water views draw people to the harbour edge. Again allowing access to the water is seen as a priority.
- Materials selection is important. The durability and textures must respond to various needs and preferences of wheeled users, skate-boarders and pedestrians.
- The area of green space or open space remains the core issue with others, such as appropriate plant selection, highlighted during the feedback sessions.
- Allowing areas for moving fast or slow, safely to be balanced with specific surfaces for various uses was seen as important.
- Destination points to activate space, add interest for all ages and user types and opportunities for rest.

Site Usage

The Honeysuckle Public Domain should be not only accessible to everyone but also engage all the various user-groups in a variety of experiences and activities. The youth focus group, for instance, demonstrated that a variety of experiences need to be provided beyond the generic cinema and skate bowl. The development of the Public Domain should provide a well balanced mix of outdoor activity, sheltered facilities and passive quiet spaces. Further there should be provision for recreational spaces that are accessible and useable at no cost to the visitor. Suggested uses include:

- Outdoor performance is intrinsic to the culture of Newcastle and Honeysuckle is seen as potentially embracing events where residential areas are not adversely affected.
- Likewise, the tradition of fishing off the wharf needs to be facilitated if not on Merewether Wharf, then at suitable places along the waterfront.
- Summer cinema and symphony-by-the-sea are a couple of the suggested events that could occur in the waterfront open space areas.
- A vital component will be a community centre, as proposed by the community in the early stages of planning for Honeysuckle.
- While the built form will accommodate exhibition spaces and entertainment facilities, periodic events such as market stalls and food fairs could be held in a number of places within Honeysuckle.
- Retention and development of Throsby Wharf as a working waterfront for both tugboats and cruise ships is important for utilization of existing infrastructure and adding life to the harbour edge.

Community Comments on Site Usage

There is no space for public performance.

Recognised clash with harbour-side apartments. Pacific Park no longer an option. Events like surf-fest are good as they bring people into the city.

'Spaces of resistance' - how will groups wanting to claim space be accommodated?

Foreshore Park is passive. Kids in Newcastle are crying out for active entertainment.

Could there be some passive recreation at no cost to families? Could it also be educational?

ENVIRONMENTAL CHANGE

Much of the red cedar that once grew in the Hunter Region either passed through here as logs or was felled by tools made and maintained here. Large deposits of shells, once metres deep along the shores of Fullerton Cove, were burnt to make lime for mortar and stored here for later use in Sydney building works.

Nobby's, once an island, was cut down to half its height. A breakwater was built to it, at great human and financial cost, so that ships could navigate more easily into the harbour. Removal of vegetation from around the gaol above Newcastle Beach resulted in sand drifts which engulfed whole buildings near the Lumber Yard. The alignment of the Lumber yard wall, however, represents the biggest landscape change.

The northern fence followed the original edge of the Harbour Foreshore, only metres from here, across what is now Bond Street. From the 1840's the railway shallows were filled in to make land for wharves and railway yards.

Exerpt from NCC signage at the North East Lookout



Floating skyscrapers

ABORIGINAL OCCUPATION...For many thousands of years the Awabakal tribal people lived along the banks of the Hunter River, enjoying the abundant fish and shellfish of the estuary. Excavations carried out on this site have revealed stone tool artefacts and also remains of an Aboriginal campsite located here. European invasion and occupation had drastic long-term consequences for the Awabakal.

Disease, violence, the exploitation of their women and the disruption of tribal culture all devastated their way of life, but these people continued to adapt and survive, living and working in the Newcastle area to the present day.

Excerpt from NCC signage at the North East Lookout



Community Comments on Site Interpretation

Create a place that can be understood - in terms of land use over time.

What existed here were small pockets of islands - how do we create an image of that?

Interpretation should not superimpose, it may suggest greater history.

Contradictions and tensions are an important aspect of the Honeysuckle site.

Need for postmodern playfulness and quirkiness. Wheeler Place is seen as too serious.

The reason I am here is because I love the way the place has been presented: the respect of the buildings has been maintained.

Hard edge to be broken is a great idea - echoes the cove idea.

We agree that you can't recreate the past.

Worth Place Park - there is no history as it is reclaimed land.

Site Interpretation

Oral histories of changing activity on the waterfront have resurfaced through the process of this Strategy. Amongst others these include:

- The fire on the ship 'British Honour' while moored off Lee Wharf;
- Migrants fishing for octopus; and
- The punt to Stockton are shared recollections.

It became clear that history is a living part of the site:

- Shipwrecks lie on the floor of the harbour;
- Ballast from historic trading ports are lodged in the breakwater; and
- Rail tracks are inlaid into the surface of wharf road.

This history extends underground to the first mine shafts of the colony. History beyond living memory is revisited by Novocastrians as they face the prospect of dramatic change.

The significance of shifting sands, disappearing islands, abundant oysterbanks of the estuary and the waterfront as a place of both Aboriginal and European industry have been spoken about and are beginning to be uncovered through revitalisation, as at Customs House. Most significant is the idea of change itself. The proposal of an historic walk could be integrated into the design development of the landscape design of Honeysuckles' public domain. This interpretive journey could also suggest connections to greater Newcastle and the Hunter Region as well as a celebration of change and the future of the city.

The drama and scale of passing ships, described as 'floating skyscrapers', is a fundamental part of the site. Lively, unstructured and unscheduled activity occurs at the waterfront throughout the day. Visibility and accessibility to significant elements such as the tugboats, connections to Nobby's headland and tidal change must be ensured through design development. While the maritime use of the harbour has continued to the present day, the railway use of the land and the reason for the reclamation, is less evident today.

Design

Community Comments on Design

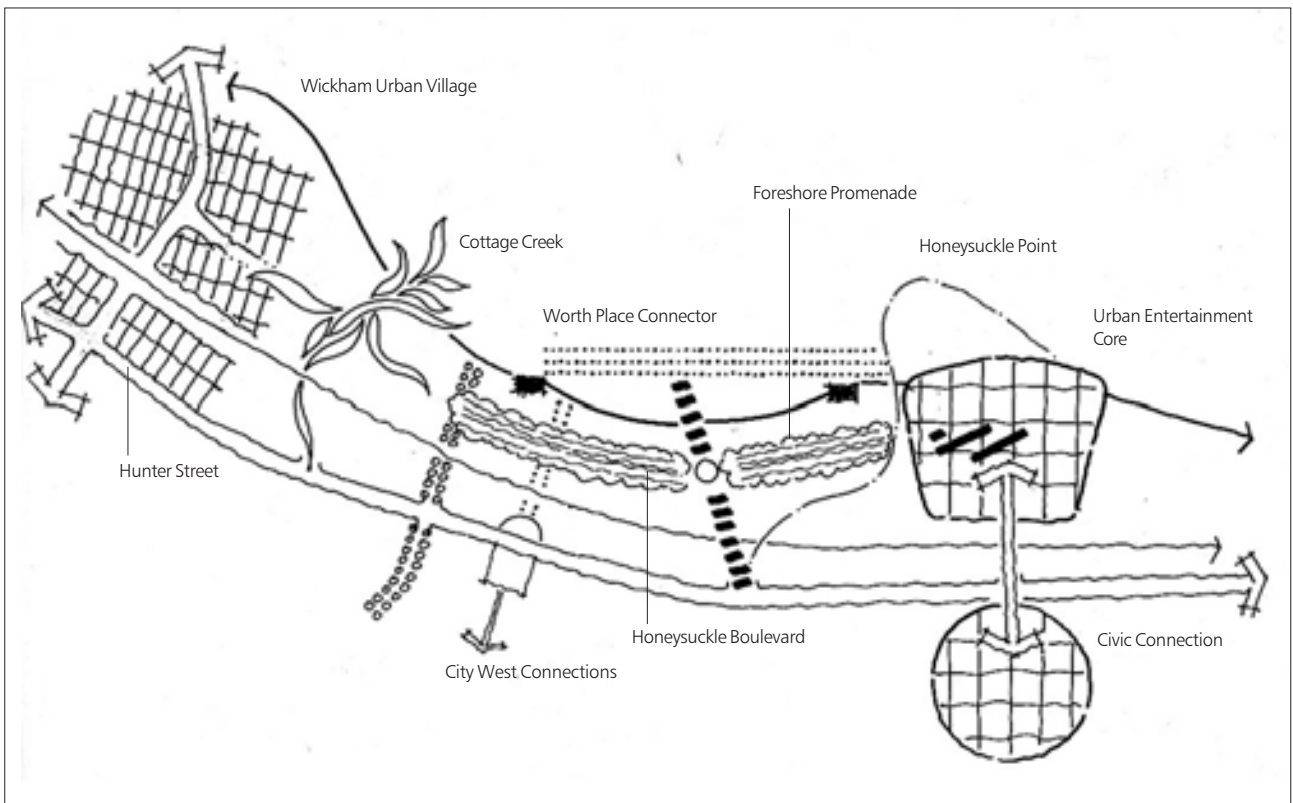
Will we still see ships tied up along this area - at Throsby No. 1?

Where the cemeteries once were still ghosts/bodies remain. Ghosts are visiting the Bellevue.

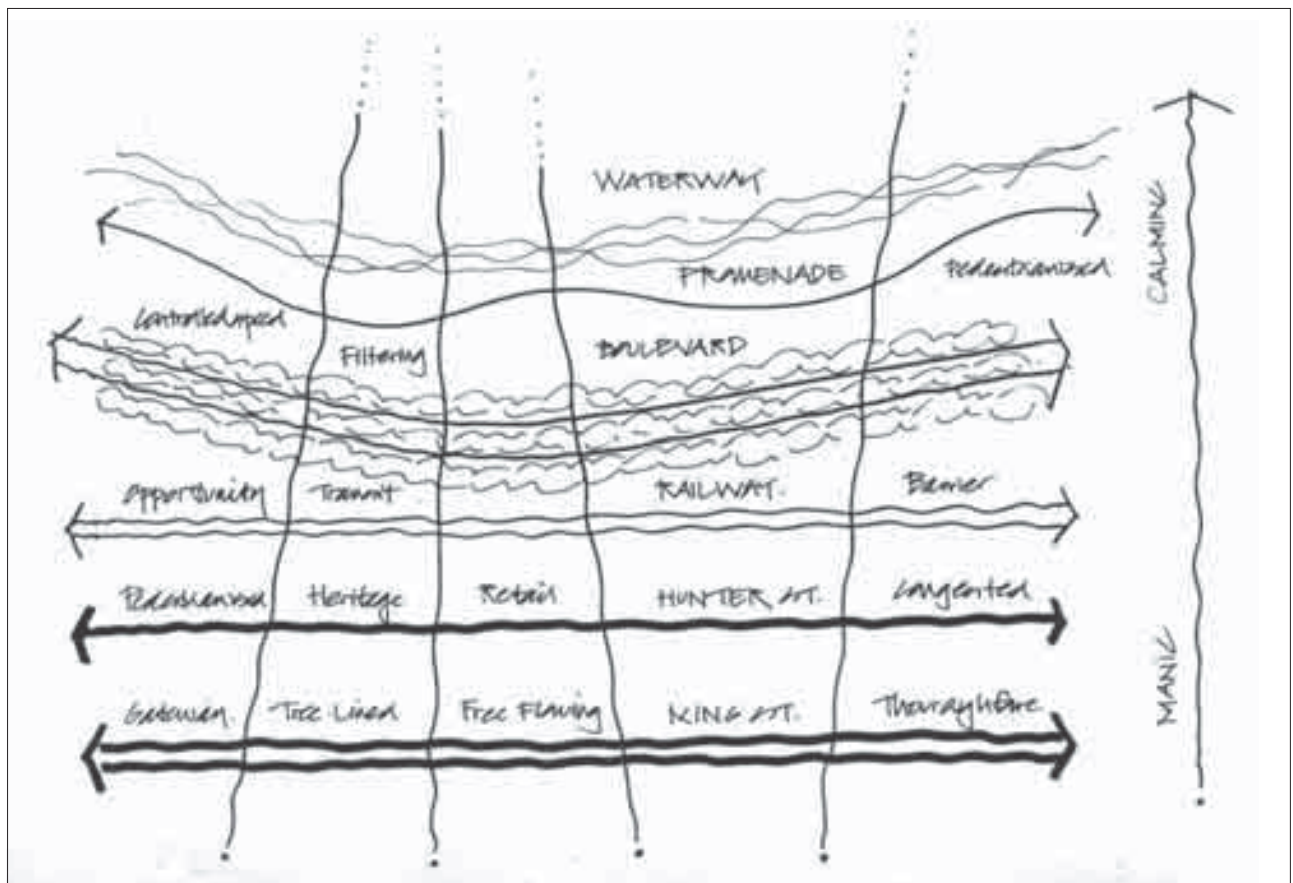
Mangroves could smell at low tide, however mangroves do not have to be a negative - they are becoming cleaner.

The community has indicated a keen appreciation and awareness of design issues including architectural style, built form, traffic and pedestrian circulation and open space design. Although the majority of design issues are beyond the scope of the strategy they should be taken on board by HDC as part of the ongoing development process. Specific issues to be considered include:

- Design of buildings associated with Urban Entertainment Centre to integrate with heritage buildings;
- Avoiding the risk of "toy-town" architecture that has occurred at other post-industrial redevelopments; and
- Relationship of Honeysuckle 'architecture' to existing fabric of the city.

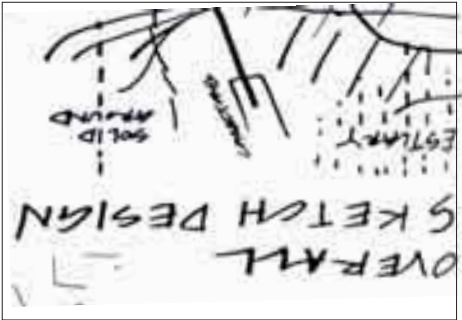


Thematic Precincts Diagram



Transition and connection

3.1 THEMATIC CONCEPTS



Sketch Ideas

The review and research phase uncovered a rich storyline of the transformation of Honeysuckle from an earlier estuarine inlet to the current post-industrial foreshore edge. The colonial settlement and later industrial processes that have shaped greater Newcastle have been instrumental in determining the present form of Honeysuckle. In particular its edge relationship with the harbour, railway barrier to the rest of the city and heritage buildings and remnant rail links are legacies of Honeysuckle's role in the export of goods and resources to global markets. Unfortunately, much of the physical richness of the industrial waterfront has been lost as the site has been cleared and prepared for redevelopment. It is the aim of the Strategy to uncover and retell the fascinating story of Honeysuckle's history through the development of the public domain.

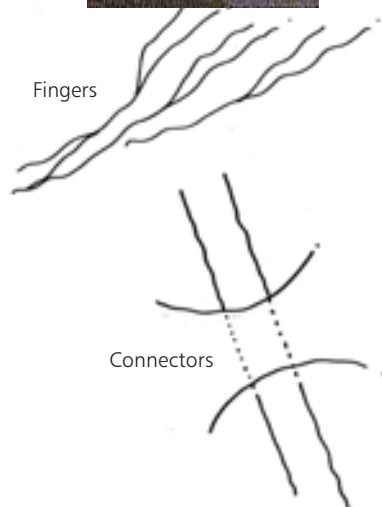
It is important that the story of Honeysuckle, the collective memory, is not lost during the phased redevelopment of individual precincts in what is now essentially a green (brown) field site.

The identification of key thematic concepts is designed to inform the redevelopment process at Honeysuckle, and in particular the development of a meaningful and layered public domain. Understandably these concepts are sometimes in conflict with the parameters outlined by DCP 40, which was developed in the absence of historical data and referencing, and the strategy has been used to test earlier ideas and thoughts.

Foreshore Edge Transformation

The original estuarine inlets of Throsby and Cottage Creeks have been radically transformed over the last century to suit the changing port requirements of Newcastle Harbour. The sequence of change has occurred in a westward progression from the earlier settlement camps at Nobby's headland up the river to warehouses at Maryville.

Significant reclamation and dredging works have transformed what was previously water/tidal mudflats into land (fill) and what was 'solid' ground into harbour. The land to water and water to land shift is a powerful metaphor for the Honeysuckle site in particular and should be woven into the storyline developed within the public domain. Opportunities exist at the location of the former Honeysuckle Point and Bullock Island point as well as the former alignment of Cottage Creek to explore the development of this metaphor further.



Fractures, Fingers and Connectors

The layering of past land uses, infrastructure and edge conditions produces an informative industrial archaeological map that highlights three key linear elements. These can be thematically summarized as:

- Fractures;
- Fingers; and
- Connectors.

Fractures are those elements that can be described as linear interventions that fracture or separate spaces such as the railway line and the former Bullock Island Bridge. The fracture metaphor also applies to the geological stratum below the site where coal seams and redundant mineshafts are fracture zones in the earth.

Fingers are those elements that can be described as linear extensions that extend spaces outward such as the coal straits, dolphin piles and railway goods line terminus.

Connectors are those elements that can be described as providing linear connections between spaces such as the railway corridor, Hunter Street and former Bullock Island Bridge.

Fractures, fingers and connectors are important design metaphors that should inform the development of linear spaces, particularly those providing the connective tissue to Civic, City West and eastern and western foreshore open space.

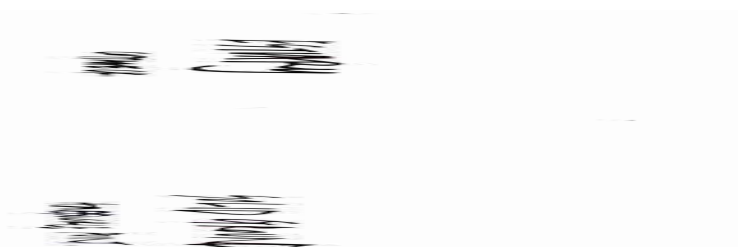
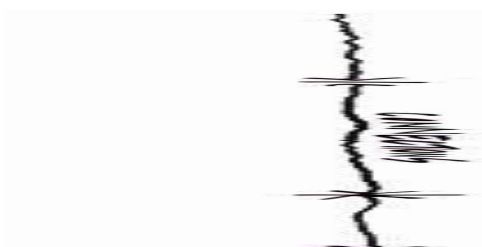
Transition and Connection

Honeysuckle will become an important place of transition and connection between the coast and estuary in the east-west plane and between the city and harbour in a north-south plane. It is important that the public domain reflects these transitions and connections through appropriate and considered planting and materials selections and detailing and spatial development.

The east-west foreshore linkage connects the coastal foreshore open space of Newcastle East through to the estuarine open space of Linwood. An opportunity exists to create foreshore open space that reflects its transitional position in the ecological succession from coastal edge to estuarine delta. Further, the urban relationship of the foreshore to the city is to be carefully considered. In particular opportunities exist to develop highly layered linear spaces that connect the foreshore edge through to the city, both in an ecological successional sense as well as an historical and urban contextual sense.



Laneways serve as important north-south connectors



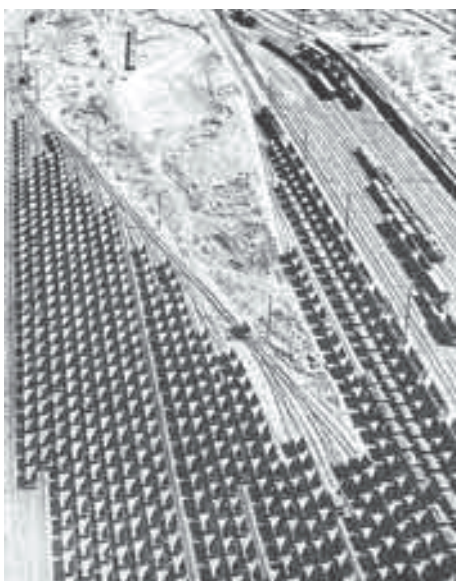
...the Honeysuckle site...is an urban redevelopment that is highly visible in terms of scale and location, but more significantly it involves a dramatic transformation of redundant industrial railyards and docklands. The renewal of this land is of particular importance because this area, which runs between the port and the CBD, has played a part in the industrial stigmatisation of the city. Consequently, 'Honeysuckle' symbolises an important transition in the purpose and meaning of Newcastle, the redevelopment representing a 'brighter' future.
p.33, Jo Hanley 1997



Berthed tugs create an ephemeral repetition on the foreshore



Remnant Lee Wharf creates a wonderful Venetian rhythm along the foreshore



Coal wagons

Repetition

A consistent theme throughout the uncovering of the Honeysuckle story has been the repetition of both permanent and ephemeral elements. The repetition of timber wharf piles creates an impressive rhythm along the waterfront when viewed from afar and up close in detail. Likewise the former railway goods lines merged and separated to create artery like traces in the landscape. The remnant pair of Lee Wharf sheds and loco workshop buildings contribute through the repetition of form, materials, colours and details.

Ephemeral elements have been key in developing the picture of a vibrant, progressive industrial waterfront. Bales of wool, coal wagons, huge timber logs, the collage of tall ship masts and rigging to the more recent line up of tugs are suggestive of the bulk movement of goods and resources on the waterfront.

Repetition of both permanent and ephemeral elements is an important design metaphor that should inform the development of both the public and private domain.

3.2 THEMATIC PRECINCTS

For the purposes of implementing a phased development program Honeysuckle has been divided up into developable precincts.

These include:

- Merewether Wharf Precinct;
- Urban Entertainment Precinct;
- Worth Place Residential;
- Cottage Creek including Wickham Urban Village; and
- Mixed uses to south of Honeysuckle Drive.

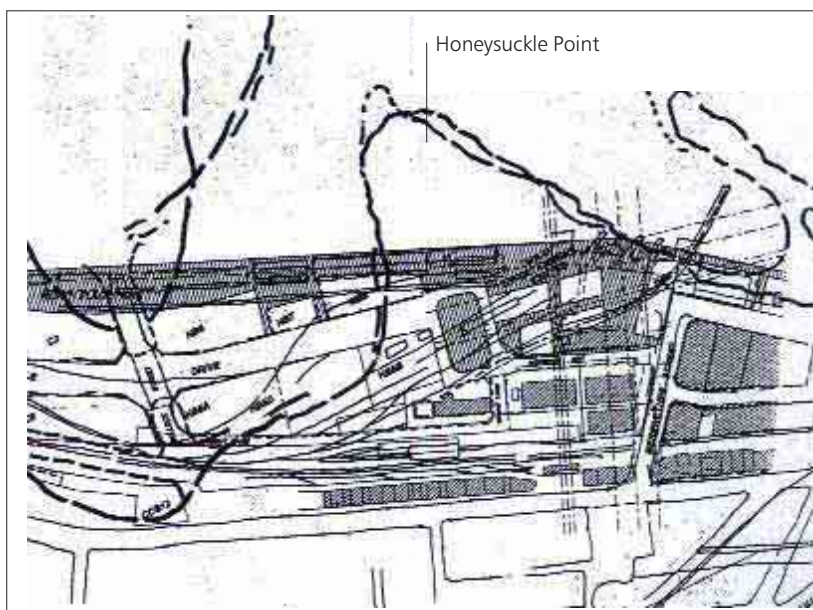


Nature city shift - hybrid forms

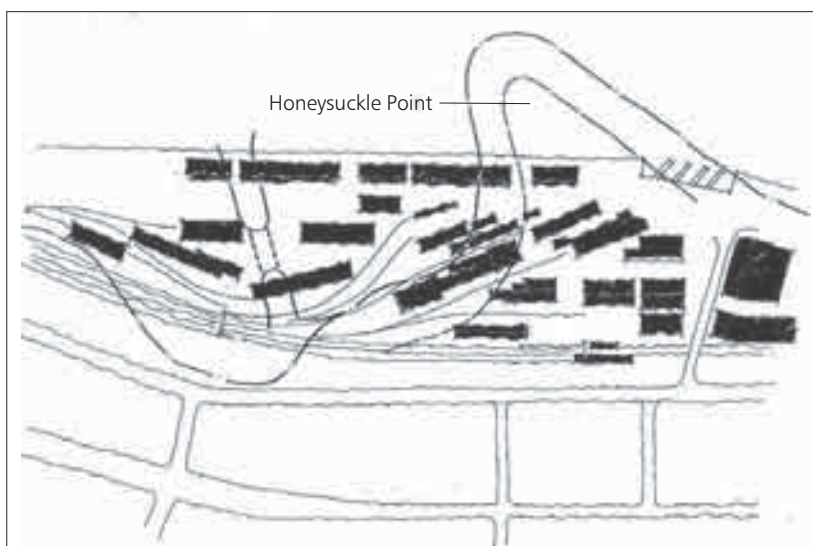
The proposed redevelopment of Honeysuckle will follow the same east-west progression along the foreshore as the original development pattern. Some precincts, namely Merewether Wharf and The Urban Entertainment Precinct in the east are well advanced in their detailed planning of proposed land use composition. At the time of writing the Strategy, expressions of interest had closed on these two projects. Others, such as Cottage Creek to the west are more vague in terms of detailed land use planning due to the anticipated lead time before they arrive on the market.

In addition, the application of core thematic ideas as conceptual overlays to the development precincts will inform the development process and establishment of a meaningful public domain. This layering will create thematic precincts with opportunities to develop the storyline into these future public places. These include:

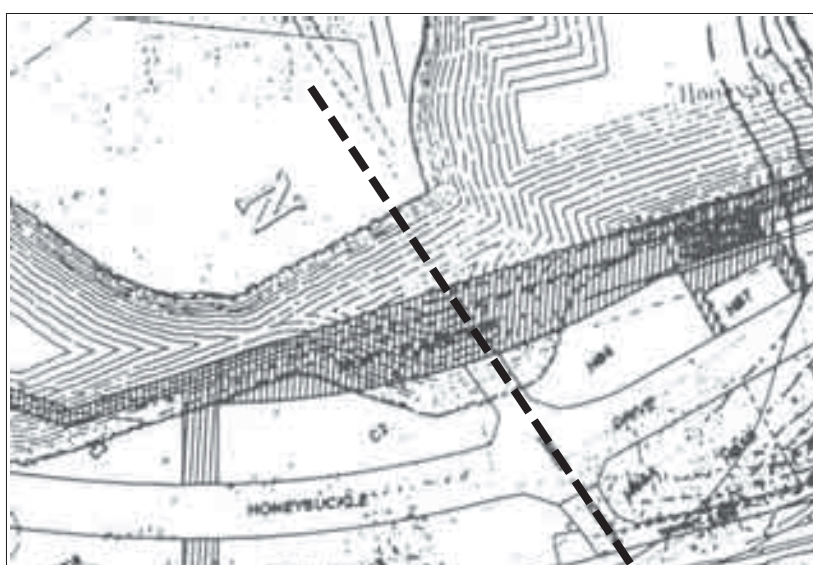
- Honeysuckle Point – Lee Wharf – Urban Entertainment Centre;
- Bullock Island Connector – Worth Place Park; and
- Cottage Creek – Throsby Wharf – Cottage Creek Precinct;



Land becomes water - water becomes land

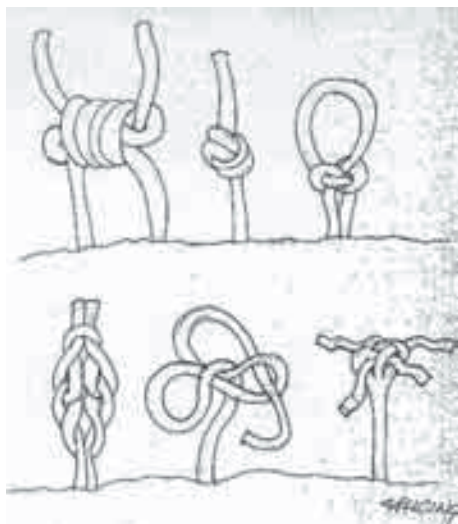


Railyard sheds like outstretched fingers on solid ground



Worth Place Connector

Each of these thematic precincts has specific opportunities to create layered places through application of appropriate design metaphor and leading ideas. This will create and identify imageability across the precincts yet ensure sufficient depth of collective meaning and content to achieved blurring of the boundaries between precincts.



Maritime Knots

Honeysuckle Point – Lee Wharf – Urban Entertainment Centre

- Land becomes water and water becomes land;
- Fingers outstretched over solid ground;
- Arrangement of railyard buildings reinforcing fingers reaching out to the world beyond;
- Intense activity of goods transitioning between railyards and port;
- A place of migrant arrival;
- Repetition of elements;
- Referencing energy and shipwrecks within harbour through steam generated bubblers; and
- Tension between natural processes and urban processes competing to establish dominance at the foreshore edge.

Bullock Island Connector – Lee Wharf – Worth Place Park

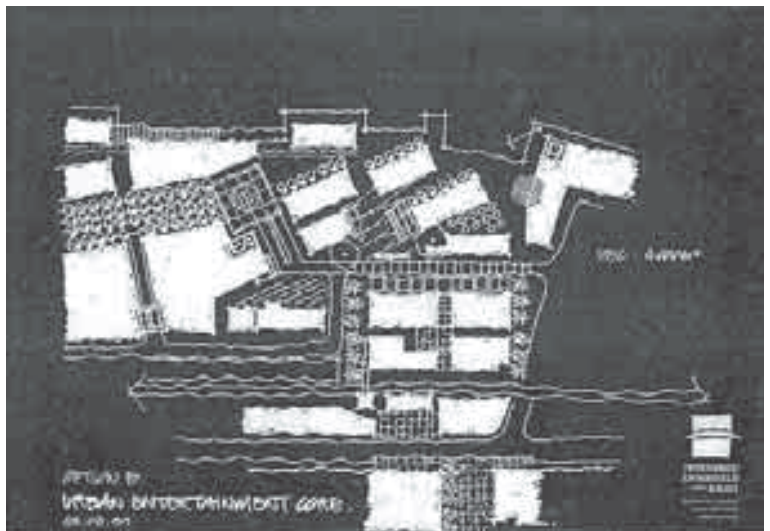
- Transient – coming and going of cargo, ships and people;
- Mass and bulk, repetition and relief;
- Tension between natural processes and urban processes competing to establish dominance at the foreshore edge;
- Breaking the edge where seawall requires replacing – nature becomes visible and accessible;
- Reference to Bullock Island connector to create fracture point within Park, a point of tension;
- Framed portals to reference connectors and ceremonial arches, gateways along North Place;
- Repetition of maritime elements – masts, sails, rigging; and
- Metaphor of knots associated with play elements.

Cottage Creek – Throsby Wharf – Cottage Creek Precinct

- Tension between natural processes and urban processes competing to establish dominance at the foreshore edge;
- Hybridized natural processes and forms to inform stormwater management;
- Remaking the creek – human ecology;
- Sustainability;
- Transience – international cruise ship terminal; and
- Transition from urban to estuarine, hard to soft.



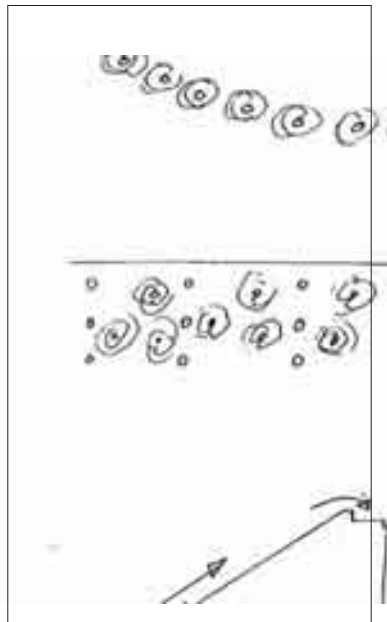
Option A



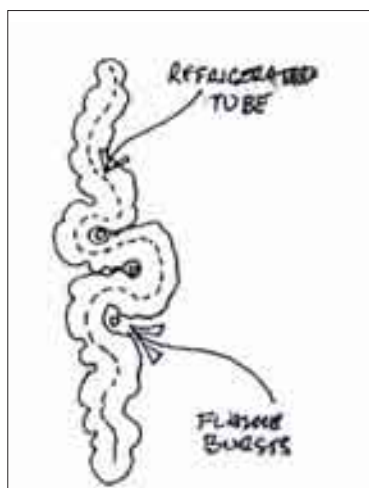
Option B



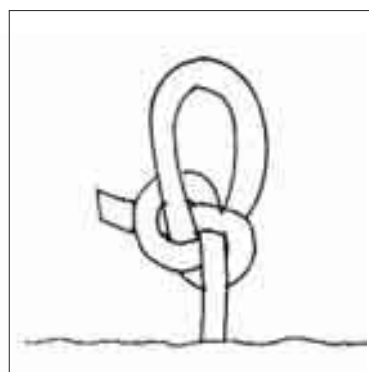
Option C



Mine Shaft



Refrigerated Wall



Knots appropriate to Marine Culture

Skating structures could be designed with the ideas of fracturing and disruption in mind – a reference to earth tremors. A variety of steps, different wall heights, slopes with broken lines could give this idea. For example, a wall which leans as if on the point of collapse.

A panel to show the contribution of excess electricity of solar and wind power generators to the electricity grid. This could be built into a wall or simply placed on the side of a building such as one of the old wharf buildings.

The use of core sample holes to access the mine shafts below. A small structure or observation point for a video monitor constructed at the ground level. To watch: A miniature video camera lowered down to the mine shaft. Solar powered lighting (and video) to listen: a contact microphone to monitor earth movements through this is too difficult to put into place. Even though the sound quality of video cameras is usually good, it is unlikely that it would pick up the subtleties heard by contact mikes.

A refrigerated wall to produce a thick coat of ice with bursts of flame issuing from points over its surface. The ice would change according to the effects of wind, sun and flames. This work could be constructed with any number of forms which could be linear or three dimensional. For example, a curving line made from a refrigerated tube which could be combined with points of fire or neon lights.

Masts, spars, chains and rigging. The masts placed in a cluster to refer to the accumulation of boats along the wharf. It could also be placed so that it is seen along a view corridor because in the past they would have been seen from a considerable distance. The sails would form awnings for shade. Some rigging for climbing.

Knots appropriate for marine culture for the children's playground. For example, bowlines, etc.

Structures to express collapse and inundation (reference to the movement of sand)

Lighting towers (solar powered) to refer to the "brown outs" as well as other historical references in the use of materials and forms. e.g. buoys, rigging, furnace panels, art deco, combinations of historic materials – wood, iron, etc.

Lighting towers that lean away from the direction of the prevailing winds.

Lighting towers with sections that move or rise up in response to the wind.

Lighting towers with heat seeking sensors so that light follows pedestrians.

3.3 ART AND SCIENCE

A radar display to register the movement of boats on the nearby harbor and other pedestrian movement.

Acoustic listening stations to assist in the awareness of ambient sounds from the water, the harbor, the wind, etc. Sound is a prominent feature of harbor activity and life as well as an opportunity to be aware of the elements such as wind and water.

1. Set into the broken section of the wharf, below deck level.
2. Large diameter pipes from near the water surface to the deck level. To enhance the sounds of the water and the harbor activity.
3. Acoustic tubes from one side of the children's playground to the other for interactivity.

Agitation of the surface of the water. A reference to the past industrious activity of the harbor.

1. Bubbles disturbing the surface in the outline of ships as they were anchored alongside the old wharf.
2. Bubbles disturbing the surface amongst the piers of the old wharf. Non-specific shape.

Lighting towers (solar powered) to refer to the "brown outs" as well as other historical references in the use of materials and forms. e.g. buoys, rigging, furnace panels, art deco, combinations of historic materials – wood, iron, etc.

The use of tidal movement through fissures which boost water movement. This could fill pools which then overflow to give a periodic waterfall.

A tidal moat around a midden of oysters. The water could be made to move like a stream with solar powered pumps or the water could be still to form a reflection pond. These pumps could also keep water levels to mimic the actual tides which operate at below ground level (otherwise the pools would be too deep).

The history of Newcastle being a place of energy, both natural and human, creativity and technical innovation suggests that the public domain becomes a place where the fusion of art and science is clearly evident and indeed a determining characteristic of the sense of place.

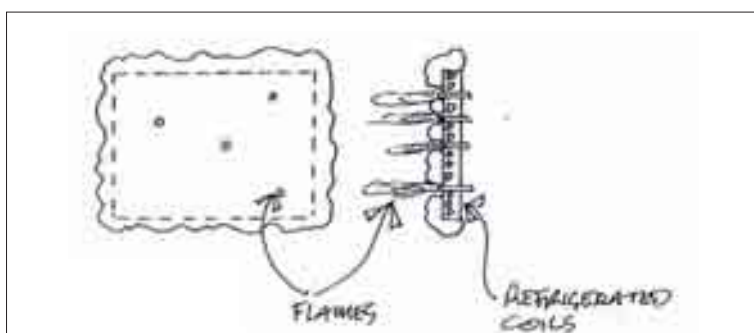
The incorporation of an integrated urban innovations program into the development of the public domain, comprising both site specific and generic installations and conceptual development is encouraged. The Strategy recommends that appropriate artists play an integral role in the development of the public domain as key members of consultant teams. Likewise, developers of individual precincts should be encouraged to include urban artists to contribute to an integrated development process.

The Strategy recognises that urban art installations are often classified as over and above bottom line budgetary projections. It is recommended therefore to overcome this constraint through carefully considered design outcomes involving urban artists from the outset. Supplementary funding could be obtained through the development of a corporate sponsorship program similar to the Illumination Newcastle program. Likewise, some form of development concession may be considered appropriate.

An inventory of ideas developed by Nola Farnen has been included to demonstrate how conceptual ideas could be implemented as part of the public domain. Thematically these ideas include:

- Form;
- Light;
- Sound;
- Fire and Ice;
- Historical metaphor; and
- Solar, wind, tidal energy.

One example of this is the use of solar and/or wind power for lighting and other electrical supply. Excess power generated could be fed into the grid to supply other parts of the city. The advantage for Honeysuckle is that any device can then plug into mains electricity with electricity generation occurring off-site. However the project would have greater integrity if all electricity generation occurred on-site with turbines and solar panels incorporated as an integrated part of the overall development.



Flames and Refrigerated Coils

3.4 CONNECTIONS

Civic Connection

The viable physical connection between the UEC and existing Civic precinct is paramount to the successful integration of the old and the new. The location of the UEC adjacent to the harbour foreshore is an undeniably attractive attribute. The challenge is to ensure that the opportunity for significant generation of public activity, retail and urban leisure infrastructure becomes a catalyst to revitalise adjacent urban areas such as Civic and City West areas. To this end the equitable and accessible connection between the old and the new, although outside the scope of the Strategy should be a medium to long term city planning objective co-ordinated by HDC and NCC.

The Strategy has reviewed previous connection proposals such as the *Urban Design Plan Newcastle Civic Area*. (Allen Jack and Cottier 1995) and concurs with the principle of the primary connection route via Wheeler Place, across a redeveloped Civic station on grade rail crossing, past the loco workshop buildings and into the UEC. The primary constraint is the existing rail line and crossing arrangement at Civic Station. The future of the station and heavy gauge rail into the city centre is key to an equitable and accessible pedestrian connection. The strategy envisages the following redevelopment scenario necessary to create this vital connection:

- Upgrading of Civic Park and renewal of civic ownership and patronage of Newcastle's 'Central Park'.
- Retro fitting Wheeler Place to create a complimentary urban space to the green space of Civic Park, both in terms of civic ownership and patronage through activated edges, appropriate spatial definition, multiplicity of uses and functions.
- Closure of Merewether Street crossing to facilitate opening of Worth Place crossing (although this has its own inherent traffic implications) allowing reconfiguration of Hunter Street signals to create a signalised pedestrian crossing zone in front of Railway Square.
- Redevelopment of the existing solid masonry structure with a more transparent permeable structure befitting of its central location and transitional context. The incorporation of a landmark structure comprising an all access lift and stairs and rampway at the western end of the station would establish a legible and accessible crossing point should heavy gauge rail remain the long term transport option into the city centre. Ideally an on grade crossing associated with a light rail station would replace inequitable grade separated crossing structures.
- The reconfiguring of access steps including an appropriate rampway system on the northern side of the station could be integrated with a new infill building associated with the workshop to activate the space between the station and the UEC. This new building would strengthen the urban form and activity around the station whilst creating a new internal workshop square space.

Light Rail

The consensus identified during the community consultation meetings was overwhelmingly towards introducing a light rail mass transit network to replace the existing heavy gauge network between Hamilton and Newcastle City Central Station. The primary driving force behind the transition to light rail is to create a more permeable connection between the old and the new. The current railway easement is a significant barrier to permeable access between Hunter Street and Honeysuckle. The creation of essentially another street along the light rail corridor would facilitate a more activated street edge on either side of the easement. Light rail also represents a tangible shift towards the creation of sustainable city with alternative equitable public transport. The strategy endorses the community consensus and recommends strategic dialogue and planning be initiated by the key stakeholders - Newcastle City Council, City Rail and HDC.

3.3 THEMATIC OPPORTUNITIES

Option A

- Retains the existing squareabout boulevard termination arrangement.
- Feature pavement to indicate transition from roadway to pedestrian shareway.
- Physical reference - of pre-reclamation transition from land (Honeysuckle Point) to water.
- Activated squareabout with landscape sculptural installations to reference ideas of tension between urban and natural edge processes.
- Loss of UEC floor space yield (1000m²)

Option B

- Replaces squareabout with open paved square termination to boulevard and connector to workshop way with median on alignment of UEC buildings.
- Positive connection between UEC and cinema, allows for drop off and turn around.
- Creates useable public domain to Lee Wharf Shed A and connective space between UEC and Lee Wharf.
- Creates more usable urban space with deletion of squareabout whilst maintaining UEC yield requirements.

Option C

- As for option B but delete connector to workshop way, effectively creating a fully pedestrianised zone within the UEC.
- Honeysuckle Drive terminates at square with limited access for service vehicles.
- Traffic diverted to southern access road along railway corridor between Workshop Way and Worth Place.

During the key stakeholder review process and the community consultation program there was opportunity to test earlier concepts and to question earlier planning decisions that had been made in the absence of historical reference. The Urban Entertainment Centre was tested in terms of the existing physical constraints and the DCP 40 Structure Plan.

Three options were developed for the purpose of discussion against the existing structure plan outlined in DCP40.

The following objectives were

- Investigate alternative boulevard termination arrangements to reduce vehicle and pedestrian conflict and to create more positive pedestrian linkage to the proposed cinema and carpark;
- Investigate alternative building envelope arrangements for the Urban Entertainment Centre which relate to historic references to create more appropriate hierarchy of public spaces; and
- Investigate appropriate physical connections to Civic
- Explore the opportunities for urban space (as against gardens or green space which can contribute to the urban fabric of this waterfront place particularly given the significant “green” space to the east and west of the Honeysuckle site.

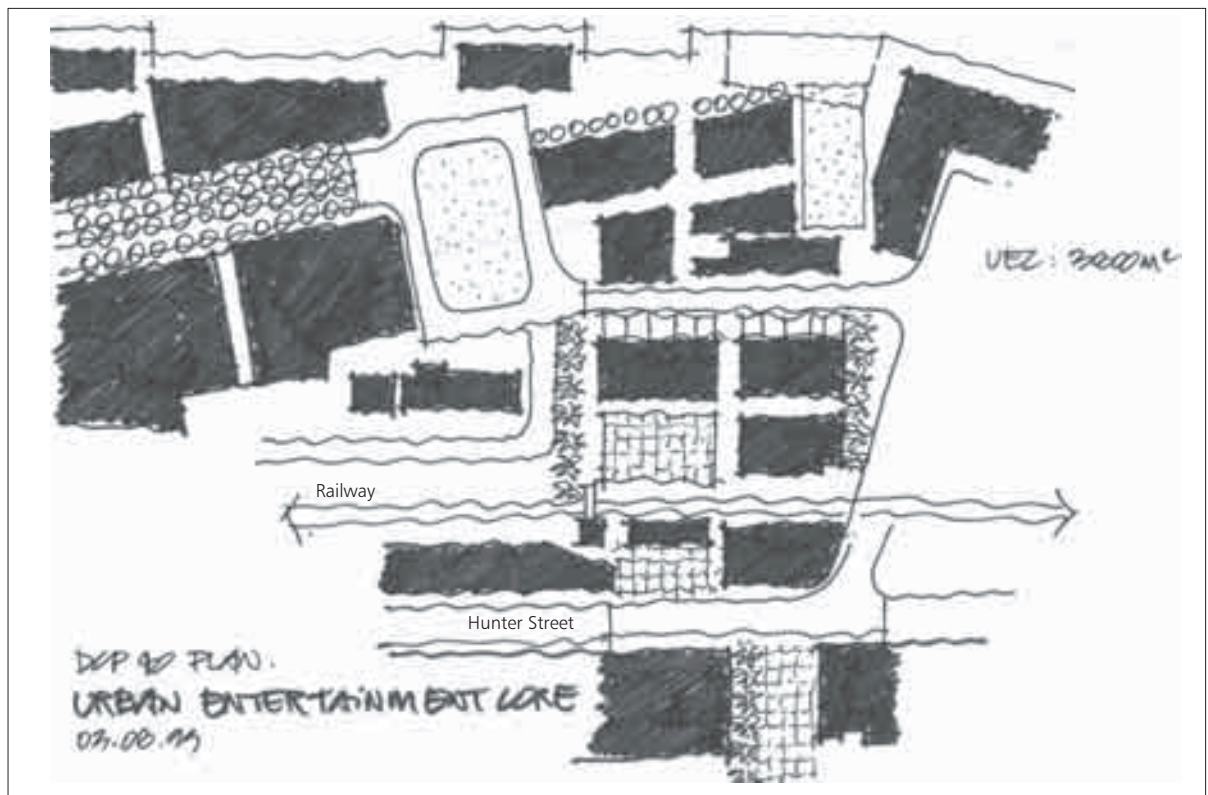
All three options realigned the Urban Entertainment Centre building envelopes, in varying arrangements, to reflect the previous alignment of the Loco Workshop buildings. Option B emerged as the preferred option and has been used to develop the precincts public domain language. Its advantages over the DCP arrangement include.

- Memory connection to the past
- an enlarged central open space with an internal focus can be created without compromising the view corridor to Nobby’s;
- existing refurbished buildings are better connected with the new urban fabric;
- greater pedestrian priority and accessibility to public domain is achieved;
- more generous views to the water from a variety of vantage points;
- a variety of connected spaces provide a range of settings and experiences;
- Provision of urban recreational space rather than green space.

Given that the buildings, spaces and vehicular / pedestrian access are geared toward public use and entertainment there would appear to be no conflict with the DCP. Uses and use areas are consistent with the current zoning, but this urban park / urban precinct will require a plan of management to be undertaken and as part of that plan special uses with the park will require negotiation. These could include:

- Cafe
- Restaurant
- Community art facilities
- Function areas
- entertainment related use

URBAN ENTERTAINMENT PRECINCT



Existing UEC Structure Plan from DCP40

*The continuous development of public space
relies upon the design of new negotiation
processes within the frame of existing political
institutions.*

Conan 1992

Vision Statement

The Urban Entertainment Centre (UEC) Precinct will become the new urban waterfront nexus for Newcastle with its central location connecting the green eastern foreshore, the civic precinct to the south and the future urbanised western foreshore. A series of dynamic places and spaces will facilitate a multiplicity of leisure, retail and cultural activities associated with the UEC precinct.

OBJECTIVES AND PRINCIPLES

The UEC shall be developed to maximise the opportunities of creating a new urban leisure core that fuses the new and the old, the east and the west and the north and the south. The development of the UEC precinct shall consider the following urban design principles:

Develop architectural language that respects character, form and typology of heritage railway buildings yet embraces contemporary ideologies, materials and retail/leisure development paradigm.

Create a place that fuses the new and the old, is capable of projecting Newcastle into the future and creates a positive uplifting vibe.

Conform to and enhance DCP40 view corridors to the harbour and Nobby's

Recognise the importance of attracting people to and containing people within this UEC precinct, ie establish a variety of spaces in terms of scale, orientation, form, furnishings, activities.

Recognise the importance of the addresses to both the harbour and the heritage railway buildings.

Ensure continuous foreshore access through the precinct.

Establish, where possible, lineages both physical and visual through to civic

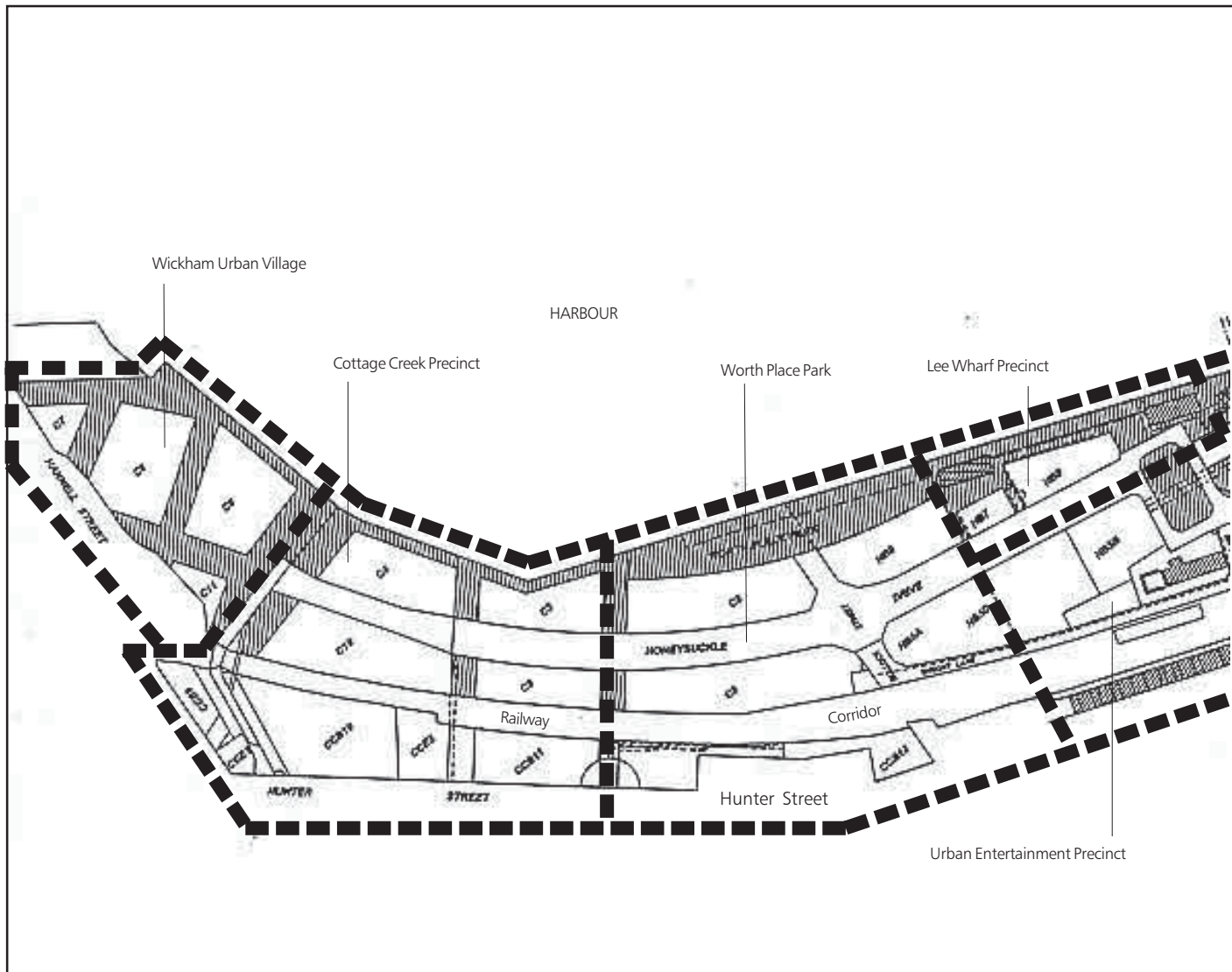
Retain and upgrade existing boat mooring cove for recreational watercraft access and modified version of the original 'cove' concept.

Develop horizontal plane to reinterpret historical layers of previous railway infrastructure in terms of the horizontal patterning of fluid rail lines, orthogonal loco workshops, robust materials.

Investigate opportunities for urban art installations that reference and interpret the importance of past European and aboriginal waterfront industrial activities.

Create opportunities for indoor/outdoor dynamic in terms of flexible built typology: folding, cantilever, opening, closing.

Provision of integrated urban furnishings such as water features raised seating, shaded lawn plinthers, walling, amenity lighting.



Public domain precincts over public domain areas shown hatched

4.0 PUBLIC DOMAIN PRECINCTS

The identification of key thematic concepts, as outlined in Section 3.0 Opportunities is designed to inform the redevelopment process and in particular the development of a meaningful and layered public domain.

For the purpose of understanding the sequence of change and for implementing a phased development program, Honeysuckle has been divided up into thematic precincts. These include:

- Merewether Wharf Precinct;
- Urban Entertainment Precinct;
- Lee Wharf Precinct;
- Worth Place Park;
- Cottage Creek Precinct;
- Wickham Urban Village;
- Honeysuckle Drive; and
- Connectors and View Corridors.

This section develops detailed strategies for each thematic precinct. Within each precinct the following areas are addressed to provide a clear direction for implementation and supported by a suggested pallet of materials.

Vision Statement

Statements set the scene for each precinct by providing understanding of the exciting direction each precinct might take.

Activities

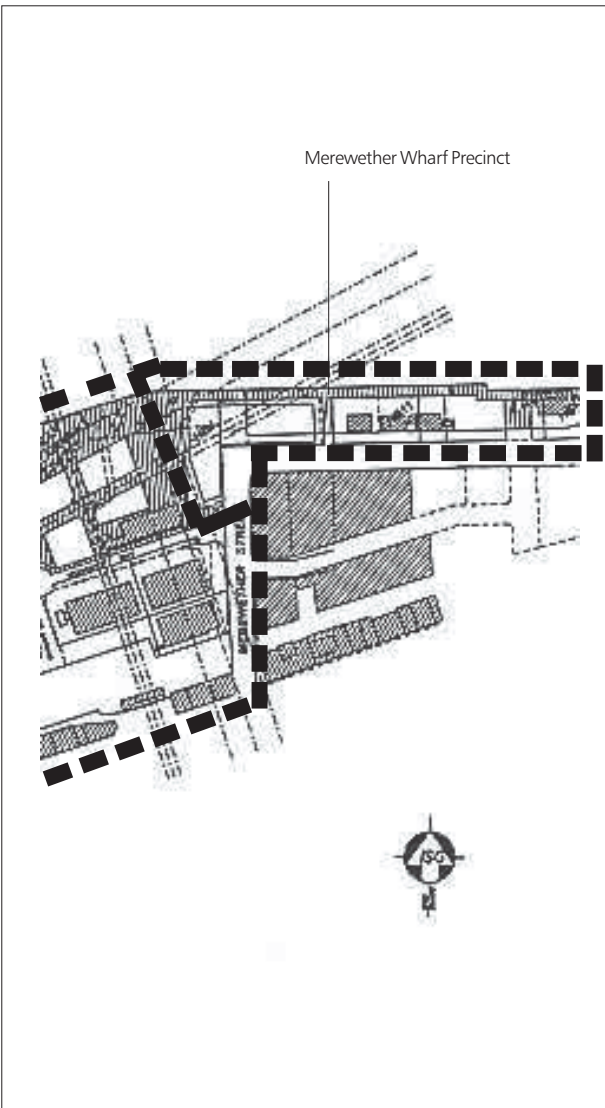
For each precinct, a collection of activities has been compiled. This suggested range of uses is indicative of the final use mix where appropriate activities are critical to ensuring a dynamic and integrated development mix.

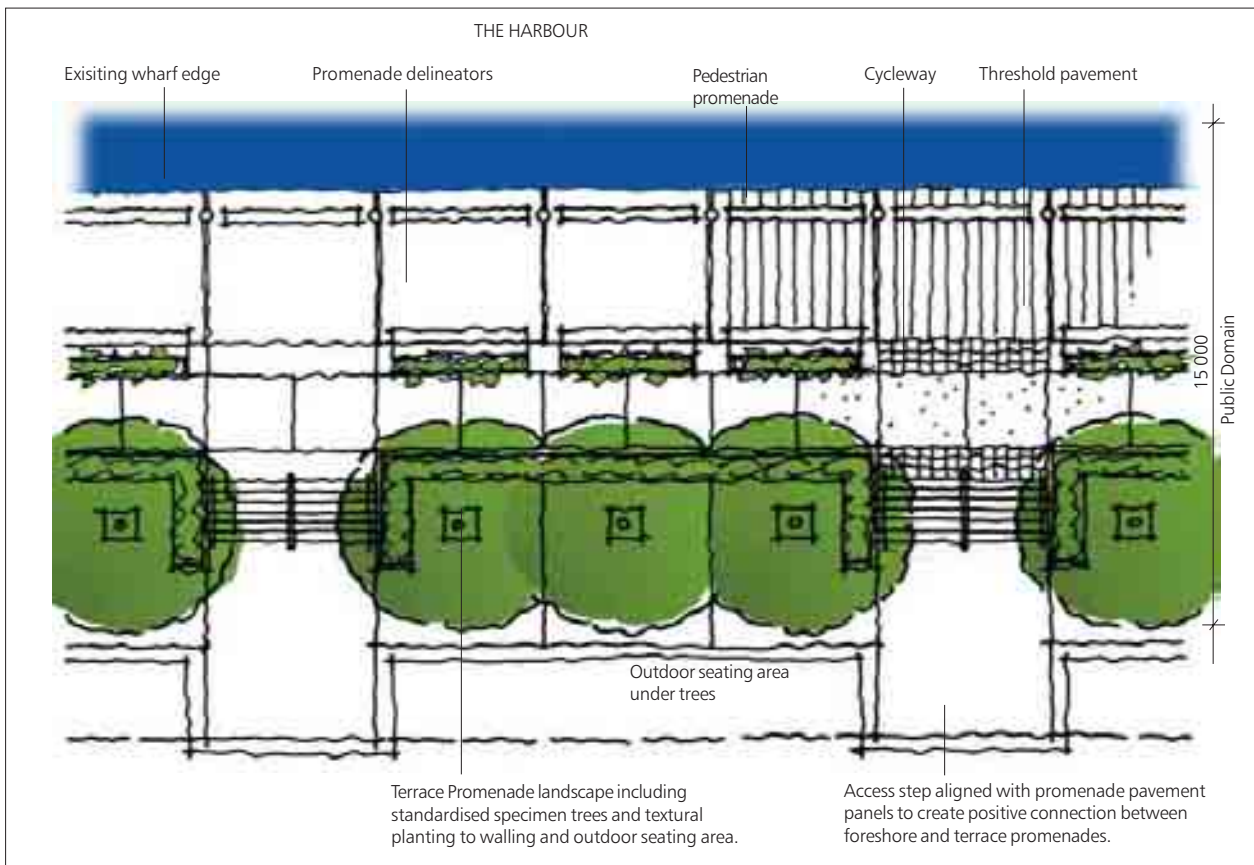
Objectives and Principles

These sections begin to identify achievable design criteria that have been developed based on urban design principles and should be considered as part of the development process.

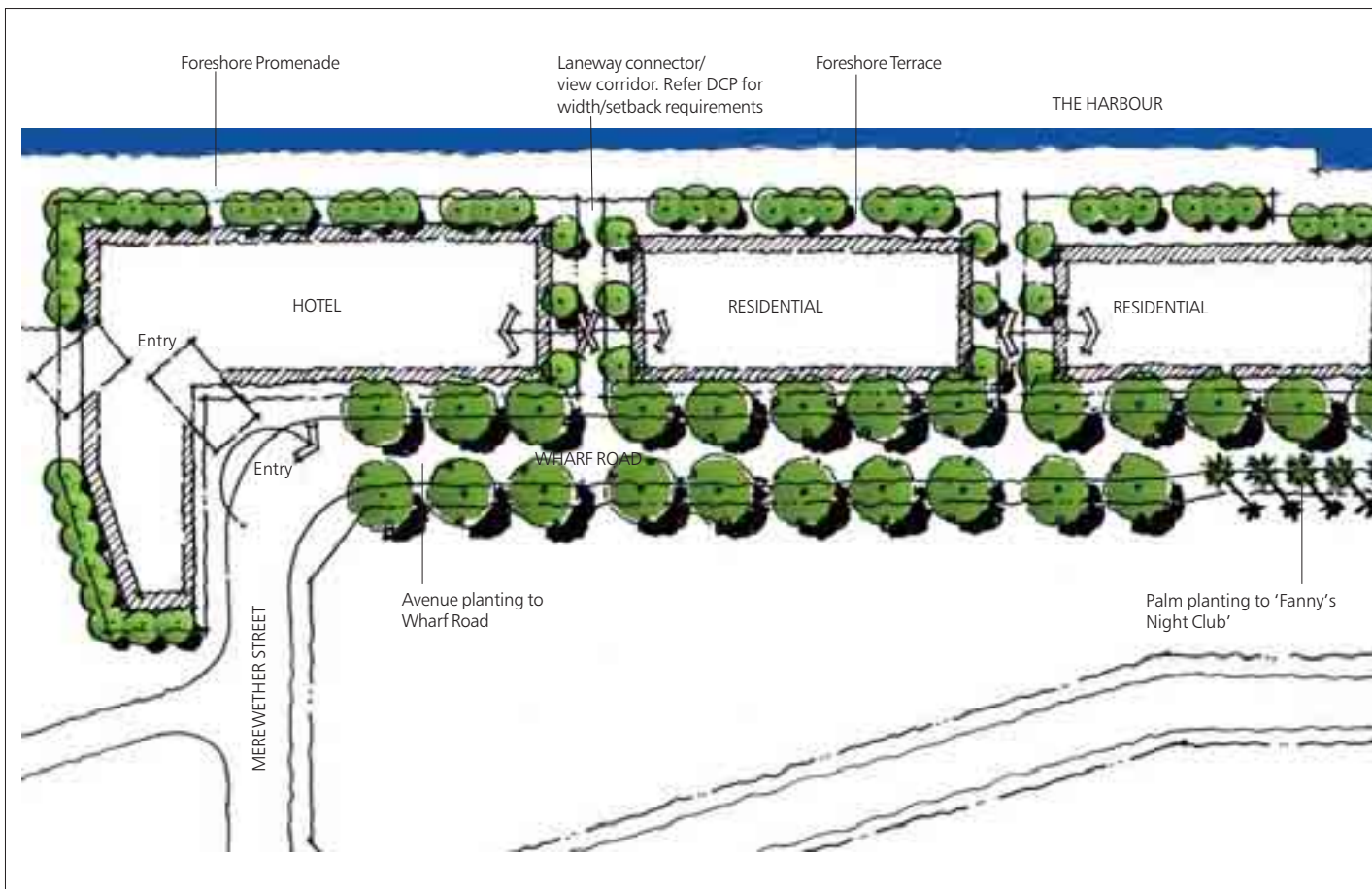
Spaces

Spaces sections identify the key progression of spaces within a precinct and the connections between adjacent precincts. An understanding of these relationships identifies the required outcomes to ensure legible and usable spaces.





Typical Detail Plan of Foreshore Promenade and Terrace

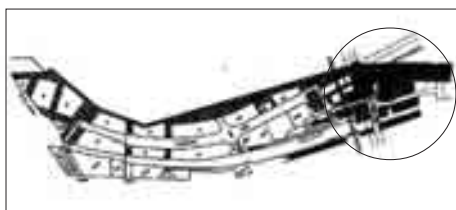


Merewether Wharf Precinct Structure Plan

4.1 MEREWETHER WHARF PRECINCT

Vision Statement

The Merewether Wharf Precinct represents a unique urban waterfront living experience in Newcastle with the proposed hotel and residential apartments creating a significant activated zone along the foreshore. The hotel in particular will create an opportunity for an iconic waterfront landmark that is worthy of international recognition.



Location - Merewether Wharf Precinct

Activities

The Merewether Wharf Precinct will comprise the following core activities:

- Public foreshore access and recreation;
- Activated zones at street level including retail and outdoor dining;
- Private hotel rooms and ancillary facilities; and
- Private residential apartments.

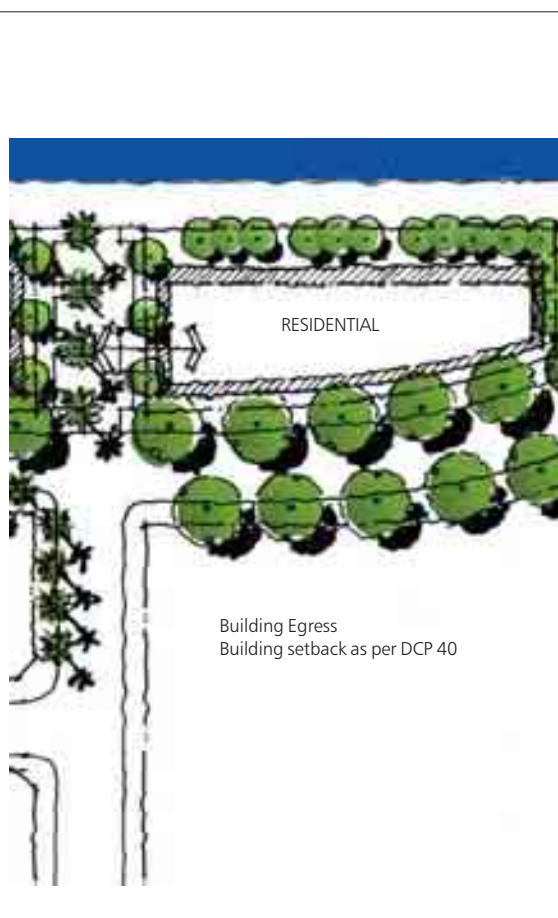
The establishment of a publicly accessible foreshore terrace creates significant opportunity for activated ground floor retail uses such as cafes and boutique shops to compliment the hotel and residential apartments above. Foreshore activities include promenading, cycling, pausing, fishing, resting, eating, etc. Furnishing of the public domain is to consider a multiplicity of uses and user groups.

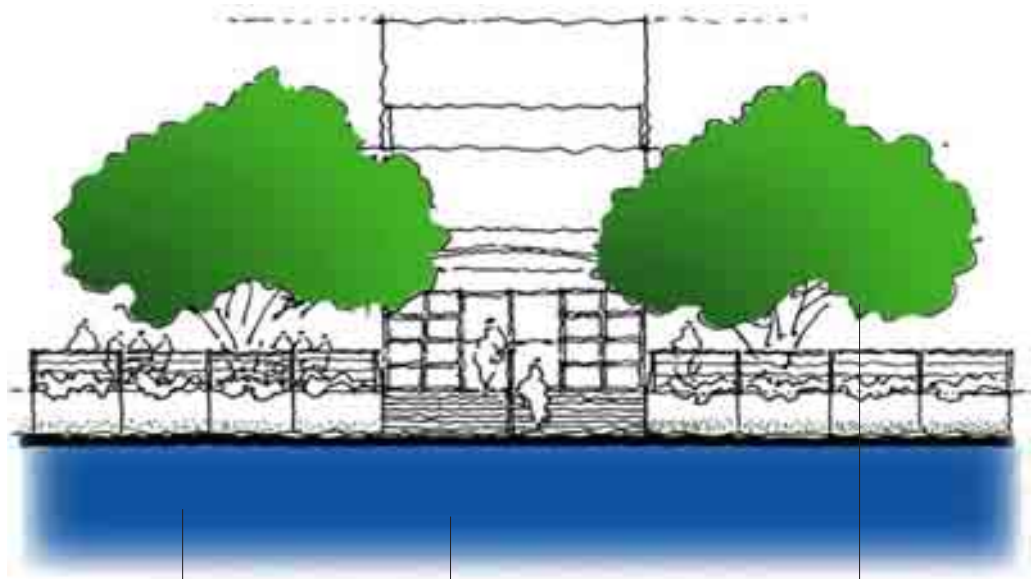
Objectives and Principles

Given its unique position on the waterfront and its connective position between Newcastle east and Honeysuckle proper, prominent urban intervention will require a creative and sensitively managed design resolution to ensure that it positively contributes to the immediate public domain and broader Newcastle Foreshore fabric.

The development of the Merewether Wharf Precinct should aim to implement the following urban design principles:

- Integrate proposed foreshore promenade into existing promenade including short term arrangements for retention of Lynch's Prawn building;
- Integrate hotel entry area and ground floor activities with proposed Urban Entertainment Precinct;
- Establish publicly accessible activated promenade terrace with appropriate activated ground floor uses along foreshore;
- Establish articulated corner treatment to north-western and southern end of hotel and eastern end of residential building;
- Ensure all access principles are implemented to foreshore terraces including ramps as to AS1428;
- Investigate opportunities for urban art installations associated with the hotel entry from the Urban Entertainment Centre;



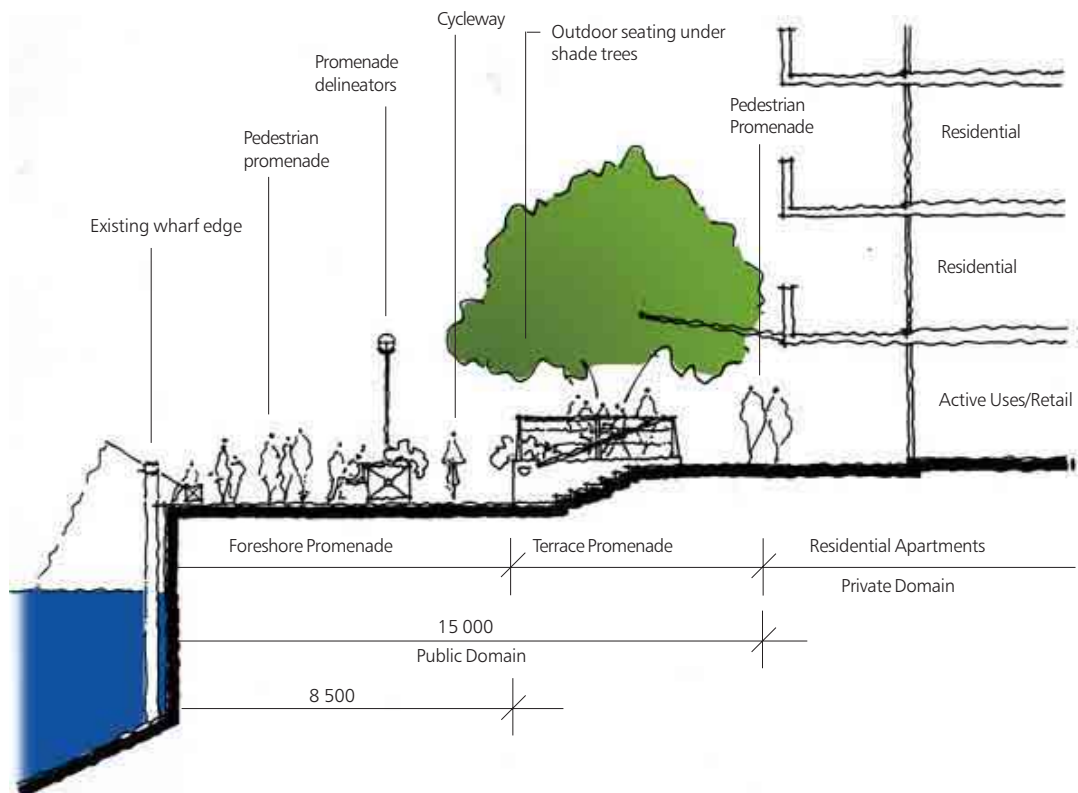


Foreshore Terrace Walling including insitu concrete panels and balustrade sections aligning with pavement panels, tidal line sand blasted into lower section of wall.

Access step aligned with promenade pavement panels to create positive connection between foreshore and terrace promenades.

Terrace Promenade landscape including standardised specimen trees and textural planting to walling and outdoor seating.

Typical Foreshore Elevation



Typical Section through Foreshore Promenade and Terrace

MATERIALS

Foreshore Promenade

Wharf Edge Treatment:

- Existing wharf edge renovated as determined by detailed structural analysis outcomes;
- Recycled wharf timber baulks min 300 x 300mm sections and 4000mm planed to remove splinters gang nailed at ends;
- Bollard lighting between baulks eg Louis Polsen Waterfront or similar.

Pedestrian Promenade:

- Hardwood timber or pre-cast concrete sections to represent boardwalk typology.

Promenade Delinators:

- Tactile pavers to threshold sections
- Customised promenade planters to incorporate planter box, seating and boundary using steel, marine ply and recycled plastics.
- Min 900mm insitu concrete steel trowelled edge strip beneath planters (non-threshold areas)
- Pole mounted luminated lighting, e.g. Louis Polsen or similar.

Cycleway

- Exposed aggregate insitu concrete to match into eastern foreshore promenade materials where possible yet read as part of the new foreshore patterning.
- Terrace level max. 1000mm, 600 preferred above promenade level.

Foreshore Terrace

- Tactile pavers to threshold sections
- Insitu concrete steps with sand blasted risers
- Insitu concrete walling with river pebble aggregate panels proportioned to represent sea wall sections and sand blasted to represent tidal patterning.
- 300 x 300mm or 400 x 400mm honed pre-cast concrete pavement with aggregate to compliment selected river pebble aggregated, feature panels recommended to delineate promenade and seating areas.
- Stainless steel or galvanised steel balustrading with stainless steel or galvanised steel railing

Laneway Connectors/View Corridors

- Refer materials section in Laneway Connectors/View Corridors
- #### Wharf Road
- Refer materials section in Primary Roads/ Boulevards

- Establish clear distinction between pedestrian and cycleway access; and
- Ensure foreshore terrace reads as the public domain and not private domain.

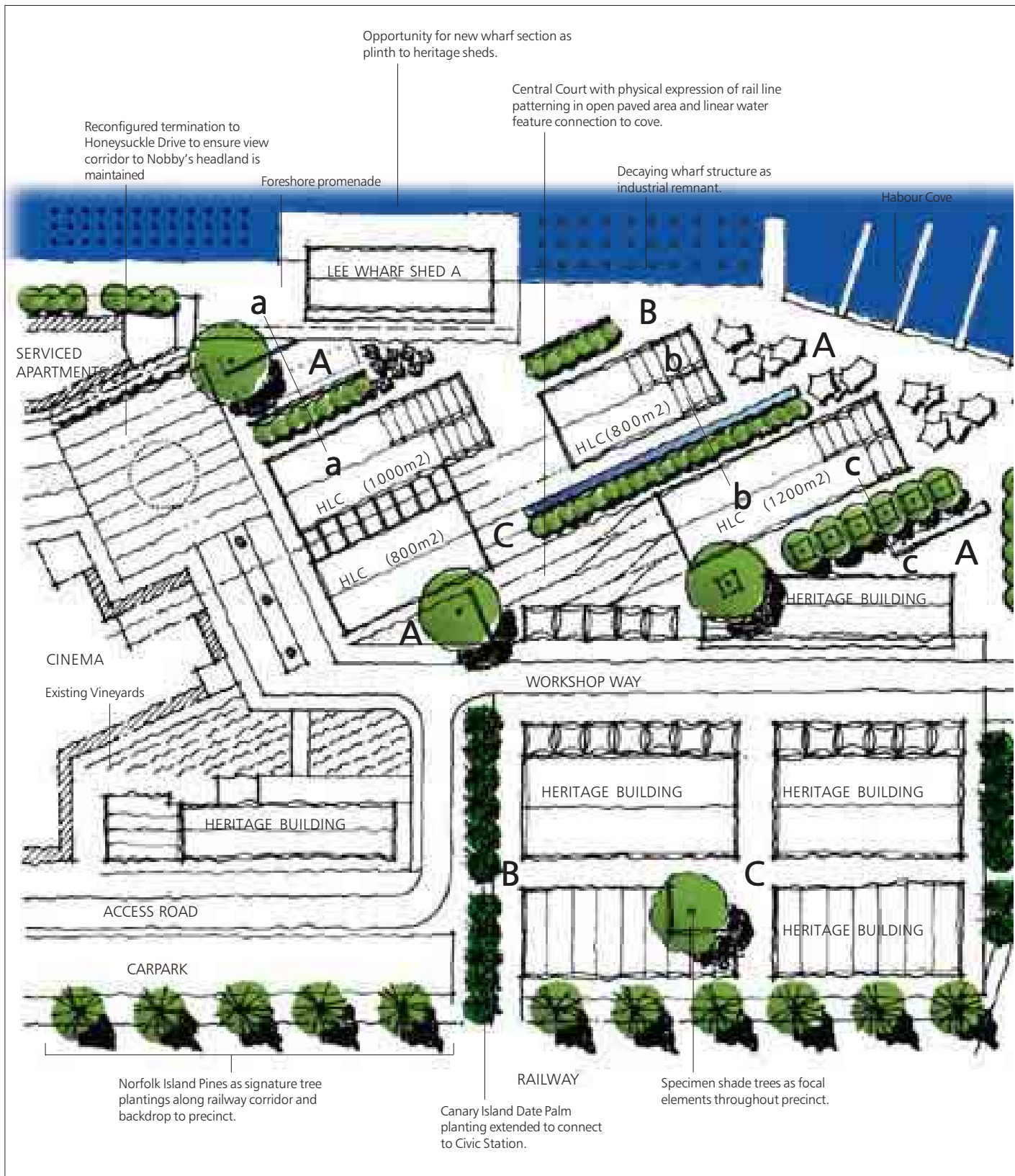
Spaces

The Merewether Wharf Precinct will comprise of a series of primary east-west linear spaces and secondary north-south connector spaces:

- **Foreshore Promenade and Terrace** provides continuation of foreshore access from existing eastern foreshore around to the proposed Urban Entertainment Centre and beyond. Specimen planting will create an appropriate green plinth to the proposed promenade in terms of environmental and visual amenity controls.
- **Wharf Road** will be modified to accommodate the Merewether Wharf Precinct and provide for pedestrian and vehicular access to the hotel and residential apartments and existing commercial activity. The road will also facilitate commuter style cycling as distinct from the recreational cycling on the promenade side which leads into the activity zone of the Urban Entertainment Centre. Avenue plantings of large evergreen trees will create an appropriate streetscape scale in the context of the height and bulk of the proposed development and reduced road pavements. Feature palms are proposed to delineate the heritage corner occupied by Fanny's Nightclub.
- **Laneway Connectors/View Corridors** provide for both visual and physical connection through to the foreshore and harbour beyond. These connective spaces are to be designed as shareways where pedestrian priority is paramount without compromising vehicular access to pavement and ongrade parking areas. Appropriately scaled street trees are to frame views to the harbour whilst softening the impact of the proposed built form and creating a suitable streetscape scale.



Concrete decking



Urban Entertainment Centre Structure Plan

4.2 URBAN ENTERTAINMENT PRECINCT

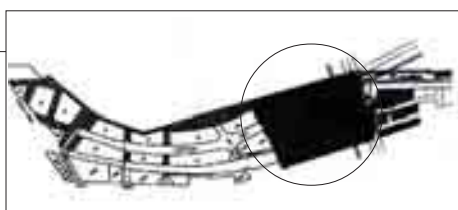
Vision Statement

The Urban Entertainment Centre (UEC) Precinct will become the new urban waterfront nexus and leisure core for Newcastle with its central location connecting the green eastern foreshore, the Civic precinct to the south and the future urbanised western foreshore. A series of dynamic places and spaces will facilitate a multiplicity of leisure, retail and cultural activities associated with the UEC precinct.

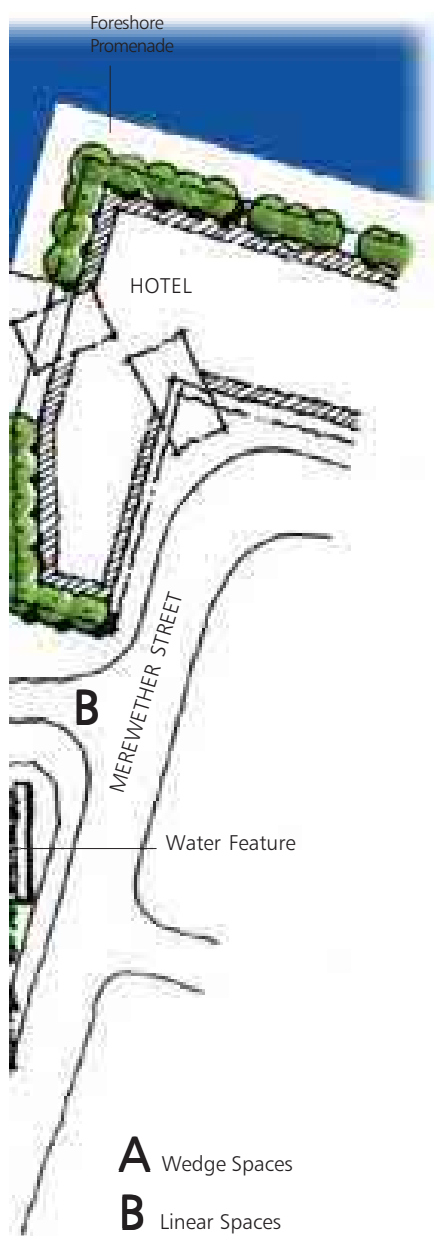
Objectives and Principles

The development of the UEC precinct must consider the following urban design principles:

- Develop architectural language that respects the character, form and typology of heritage railway buildings yet embraces contemporary ideologies, materials and retail/leisure development paradigm;
- Create a place that fuses the new and the old, is capable of projecting Newcastle into the future and creates a positive uplifting vibe;
- To be cognisant and considerate of view corridors to the harbour and Nobby's headland as identified in DCP 40;
- Recognise the importance of attracting people to and containing people within this UEC precinct, ie establish a variety of spaces in terms of scale, orientation, form, furnishings, and activities;
- Ensure continuous foreshore access through the precinct.
- Establish, where possible, linkages both physical and visual through to Civic;
- Retain and upgrade existing boat mooring cove for recreational watercraft access and modified version of the original 'Cove' concept;
- Develop horizontal plane to reinterpret historical layers of previous railway infrastructure in terms of the horizontal patterning of fluid rail lines, orthogonal loco workshops, and generally robust industrial materials that are appropriate to the retail setting;
- Investigate opportunities for urban art installations that reference and interpret the importance of past European and Aboriginal waterfront industrial activities ie. oyster cones, ice making. This could also be included as a signature element to the hotel entry;
- Create opportunities for indoor/outdoor dynamic in terms of flexible built typologies: folding, cantilevering, louvring, opening, closing;
- Provision of integrated urban furnishings such as water features, raised seating, shaded lawn plinths, walling, and amenity lighting; and
- Establish positive legible connections from UEC core to peripheral zones including proposed cinema, Lee Wharf Shed A and Loco Workshop.



Location - Urban Entertainment Precinct

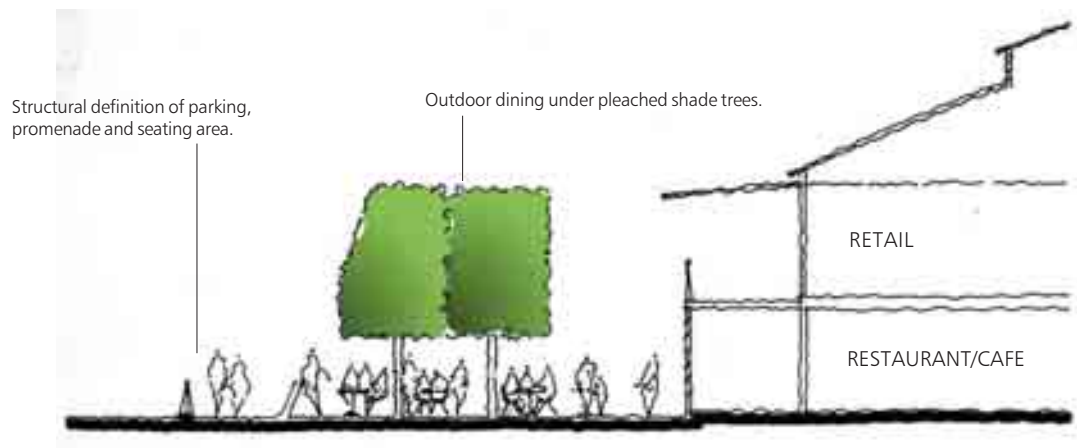


A Wedge Spaces

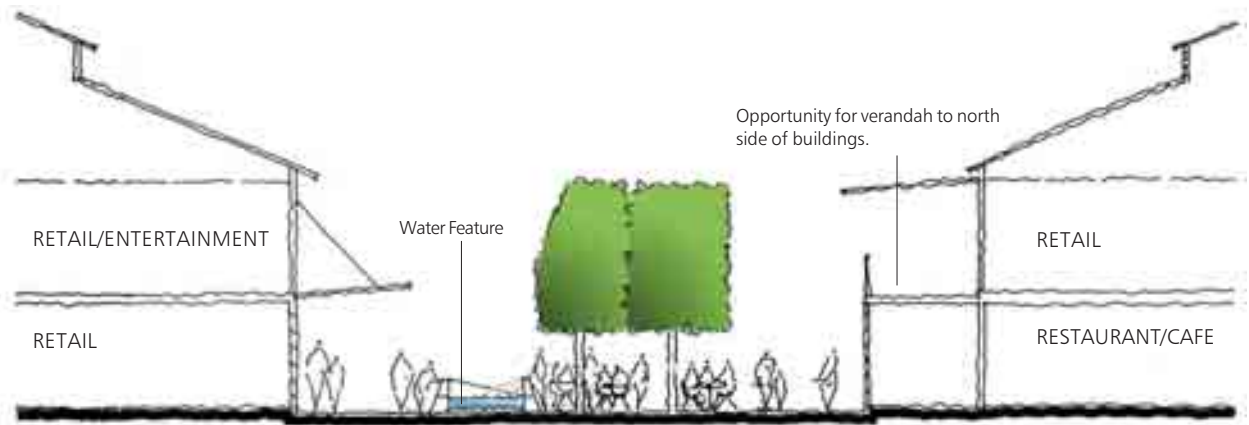
B Linear Spaces

C Central Spaces

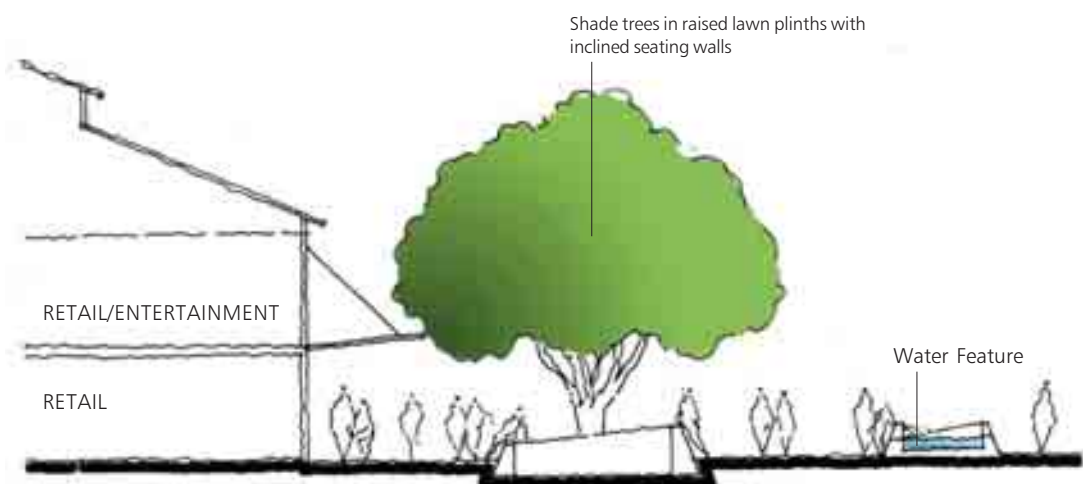
Refer following pages for typical sections



Typical section a.a through outdoor dining area addressing foreshore promenade



Typical section b.b. through outdoor areas between buildings



Typical section c.c through outdoor area addressing hotel and heritage building

MATERIALS

Pavement

- Description:
Uniform palette throughout precinct using honed insitu or precast pavement min unit size 300 x 300mm.
- Aggregate:
Denman River pebble or similar.
- Colour:
Light sandy tones to contrast with of civic precinct.
- Features:
Galvanised steel sections to reference rail line patterning.
- Detail:
Contrasting tones/banding to detail areas such as leased alfresco zones.

Walling

- included insitu concrete walling @ 5 degrees to vertical away from harbour (check) and raked down at 5 degrees to horizontal toward harbour.
- minimum section length @ 5m with minimum opening @ 2m.

Inclined Lawn Plinths

- insitu concrete walling included at 5 degrees to vertical with 5 degrees slope on lawn toward harbour.
- minimum height 400mm and maximum height 900mm.
- couch lawn on imported topsoil minimum depth 900mm for tree planting.
- subsoil drainage.

Water Features

- inclined off form insitu concrete walling as per inclined law plinths.
- water feature to include both dynamic (jets and spitters) and static water.
- incorporate opportunities for water play, eg the entrance foreshore.

Shade Structures

- Workshop Way:
To match existing structures.
- Foreshore:
Clustered structures to reference lifeguard shade structures, skeletal expressive forms inclined away from prevailing NW and NE winds.
- Painted steel posts and frame with stretched shade fabric.

Activities

The UEC precinct will comprise the following core activities:

- Retail and leisure activities such as restaurants, cafes, food court, tavern, and ancillary boutique stores to UEC core;
- Multiplex cinema complex with potential for ancillary retail and leisure activities to west of UEC core;
- Cultural activities within heritage buildings including existing Wine Society tenancy in Loco Workshop and potential future uses including markets, functions, conferences, visual arts as well as the Maritime Museum within Lee Wharf Shed A; and
- The hotel, whilst considered part of the Merewether Wharf Precinct is integral to the success of the UEC precinct in terms of spatial containment and contribution to the activities mix.

The public domain will be instrumental in ensuring that these core and peripheral activities are synergistically integrated to create an intense waterfront experience.

Spaces

The UEC precinct will comprise a series of linear spaces that open out to the harbour, a series of wedge spaces defined by proposed and existing built forms and a central square enclosed by proposed UEC built form.

- **Central Court** is to be the primary gathering and arrival space defined by the arrangement of UEC buildings. The spatial arrangement of the square is to include:
 - address to Civic Precinct connector running from station crossover adjacent to heritage building.
 - permeable connection to Workshop Way
 - view corridors to harbour to north and north-east.
 - opportunity for intense gatherings associated with occasional performances, leased alfresco dining and public alfresco dining associated with food court/takeways.
 - physical expression and interpretation of previous rail line patterning in pavement detailing; and
 - protection from prevailing winds, summer heat and UV radiation, coastal showers; and
 - linear connection between Court and Cove using a linear water feature and shade tree plantings.
- **Workshop Way** will become an important transitional space between the UEC and Civic connection. An opportunity exists to introduce a free standing shade structure element on the northern side to compliment existing structures that spatially define the linear Workshop Way and orthogonal central court spaces. A large fig tree in a raised lawn wedge will provide essential shade and act as a landmark meeting point.



Exposed detailing

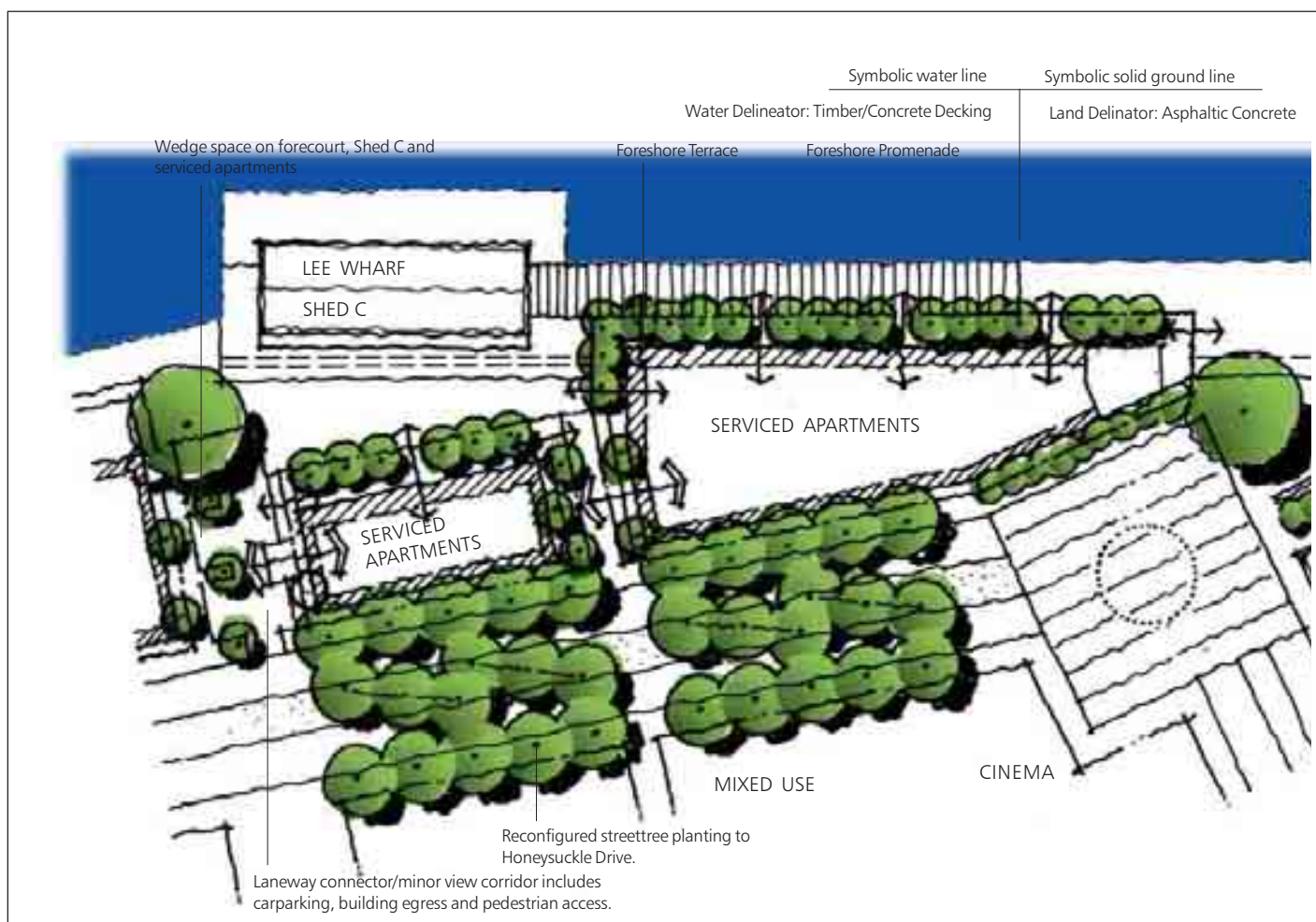


Sandblasted off-form concrete and steel

- **Linear spaces** adjacent to and between UEC buildings are to be defined by pleached specimen tree planting, insitu concrete walling, raised lawn plinths and water features. The spatial arrangement of these elements is to include:
 - legible pedestrian movement adjacent to buildings.
 - alfresco dining opportunities under specimen trees.
 - linear definition of spaces by buildings and vertical interventions including insitu concrete walling, water features, lawn plinths.
 - accentuate view lines to the harbour.
- **Harbour Cove** is to become the primary waterfront space within the UEC precinct, serving as an important transition zone between the foreshore promenade, the UEC and the hotel. The spatial arrangement is to include:
 - accessible and legible foreshore promenade zone;
 - provision of sculptural fabric shade structures suitable for casual seating, alfresco dining, informal gatherings and water play; and
 - positive indoor/outdoor relationship to UEC buildings addressing the space.
- **Wedge spaces** will be created by the juxtaposed built forms of the old and new development patterns. The spaces are to function as important peripheral spaces that anchor the entire precinct together. Proposed functions include:
 - visitor parking and shared way to the Lee Wharf Building A;
 - shaded lawn meeting place at the Workshop Way/Central Court transition; and
 - secondary arrival/gathering space off Merewether Street associated with junction of UEC, heritage building (possible use for outdoor functions) and hotel. The provision of shade trees in raised lawn plinths and sculptural water feature set in a clean paved area is recommended.



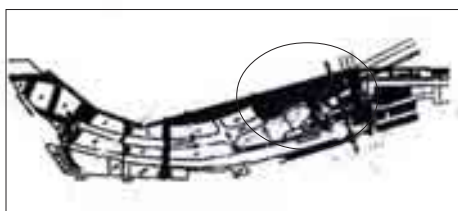
Historic referencing in materials



Lee Wharf Precinct Structure Plan

4.3 LEE WHARF PRECINCT

Vision Statement



Location - Lee Wharf Precinct

The Lee Wharf Precinct will become an important supplementary foreshore precinct to the primary UEC precinct in much the same way as the Merewether Wharf Precinct. Like the Merewether Wharf Precinct it will take on its own unique character given the adaptive reuse of the Lee Wharf sheds and proposed serviced apartment developments to activate foreshore terrace zones.

Activities

The Lee Wharf Precinct will comprise the following core activities:

- Public foreshore access and recreation;
- Serviced apartments with potential for activated ground floor, eg retail and outdoor dining;
- Cultural activities associated with Lee Wharf Sheds A and C including possible tenancies by the Maritime Museum and Rowing Club; and
- The inclusion of a publicly accessible foreshore terrace creating a significant opportunity for activated ground floor retail users such as cafes and boutique shops to compliment the serviced apartments above and cultural adaptive re-use of the Lee Wharf sheds. This will ensure a more dynamic and engaging promenade experience.

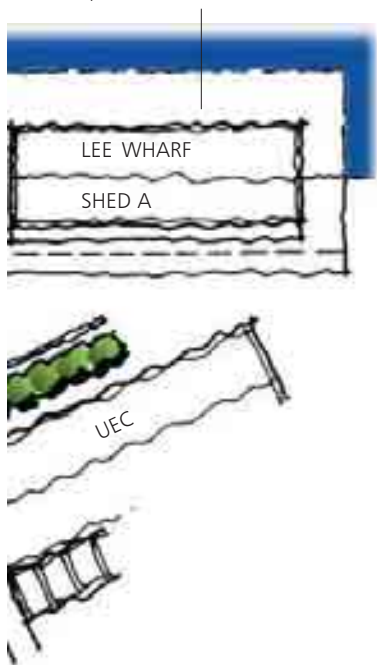
Objectives and Principles

The primary objective is to ensure that the proposed development is a creative and sensitively managed design intervention that maintains and enhances the integrity of the Lee Wharf sheds and adjacent wharf structure composition.

The development of the Lee Wharf Precinct is to consider the following urban design principles:

- Recognise the development of new buildings as contributing to overall foreshore composition of the precinct in terms of height, built form, typology, articulation, colour and material selection;
- Establish articulated corner treatment to eastern end of new development and consider relationship to Honeysuckle Drive terminus.
- Establish activated publicly accessible promenade terrace along foreshore and Lee Wharf wedge.
- Incorporate drop off zone as part of Honeysuckle Drive terminus;
- Incorporate, where possible, existing rail lines associated with Lee Wharf buildings;
- Reference previous land edge of Honeysuckle Point with change in surface finish to foreshore promenade;
- Establish clearly defined pedestrian and cycleway access along promenade;

Curtilage to shed defined with new wharf and pavement



MATERIALS

Lee Wharf Curtliages Pavement

- asphaltic concrete (AC10);
- finished pavement levels to be designed to accommodate retention of existing rail lines; and
- adjacent pavements to delineate edge of curtilage pavement.

Wharf Reconstruction to front of Sheds A and C (to be confirmed)

- reuse existing timber piles headstocks and girders where possible; and
- new hardwood timber where possible.

Lighting

- feature lighting of heritage buildings to be considered as part of illuminating Newcastle program;
- amenity lighting to be incorporated as part of building lighting; and,
- freestanding pole lighting to be avoided in preference to bollard and inground lighting to minimise visual clutter around heritage buildings.

MATERIALS (cont)

Foreshore Promenade Pedestrian promenade

- Land Delineator: Asphaltic concrete (AC10)
- water Delineator: Hardwood timber or pre-cast decking
- finished promenade level to match into existing Lee Wharf curtilage levels.

Edge Treatment

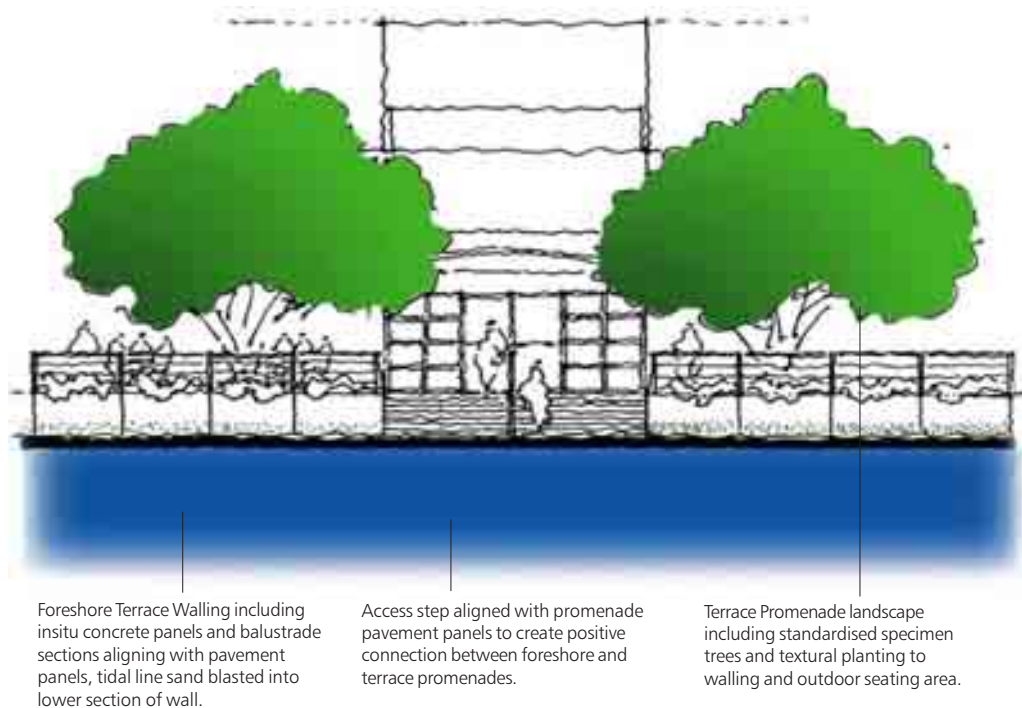
- recycled wharf timber baulks minimum 300 x 300mm sectors @ 4000mm all round to remove splinters and other obstruction, gangnailed at ends.
- inground uplighting between baulks.

Promenade delineators

- bollard lighting, eg. Louis Polsen waterfront or similar.

Seawall Reconstruction

- Land Delineator: Concrete gravity wall to existing foreshore edge alignment.
- Water Delineator: Concrete gravity wall or grout injected mattress set back from existing foreshore edge sufficient to be concealed by boardwalk above.



Typical Foreshore Elevation

MATERIALS (cont)

Foreshore Terrace

Pavement

- tactile pavers to threshold section adjacent to promenade and steps;
- insitu concrete steps and landings with sandblasted risers to steps; and
- honed insitu or pre-cast concrete pavement (300x300mm or 400x400mm) with aggregate to compliment selected river pebble, eg. Denman River pebble or similar. Feature banding to delineate terrace, seating and building areas.

Walling

- insitu concrete walling with river pebble aggregate sandblasted lower section to represent tidal patterning on seawall below; and
- walling to be proportioned to reference existing seawall panels using cast in beveled joints and expansion joints.

Handrail and Balustrading

- stainless steel or galvanised steel balustrading with stainless steel staywire or galvanised steel railing.

Lighting

- limited to inground uplighting to specimen trees and feature wall mounted lighting to building and walls

Lee Wharf and Wedge

Pavement

- honed pre-cast concrete units eg Ecoloc or similar permeable pavement to compliment foreshore terrace pavement and shared way pavement.
- incorporate feature banding or tactile pavers to delineate designated parking areas and pedestrian access ways.

Delineators

- customised bollards similar to Workshop Way bollards to be used in concert with detailed paving to delineate parking access ways in preference to raised kerbing.

Lighting

- amenity lighting to integrate with proposed lighting to Lee Wharf Shed C curtilage, laneway connectors and bollard delineators - pole mounted luminaries on alignment of proposed serviced apartment building, eg., Louis Polsen or similar.

Laneway Connectors / View Corridors

- refer materials section in *Laneway Connectors / View Corridors*

Honeysuckle Drive Interface

- refer materials section in *Honeysuckle Drive Streetscape*

- Establish clearly defined shareway for pedestrian/cyclists and visitor parking to Lee Wharf Shed C;
- Establish defined plinth to Lee Wharf sheds to separate old from new;
- Investigate opportunities for wharf decking reconstruction associated with Lee Wharf Sheds A and C as part of adaptive re-use design; and
- Investigate opportunities to retain wharf structure in short - medium term and maximising public safety, restricted access, etc.

Spaces

The Lee Wharf Precinct will comprise two primary foreshore spaces and two connector spaces:

- Foreshore promenade and terrace creates an important physical and visual linear connection between the two Lee Wharf buildings as well as an opportunity for a fracture point referencing previous land-water edge.
- The Lee Wharf sheds A and C curtilages is designed to spatially define the heritage buildings and reinforce remnant rail linear patterns as a horizontal composition yet allow a seamless integration with the foreshore promenade.

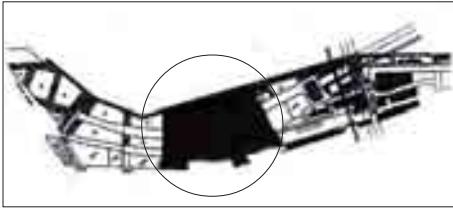


Worth Place Park Structure Plan

4.4 WORTH PLACE PARK

Vision Statement

Worth Place Park will become a significant foreshore public park that occupies the central position along the foreshore open space system between Nobby's headland and Carrington. The park will facilitate a wide range of recreational experiences and engage park users on a variety of sensory and participatory levels that is unique to Newcastle.

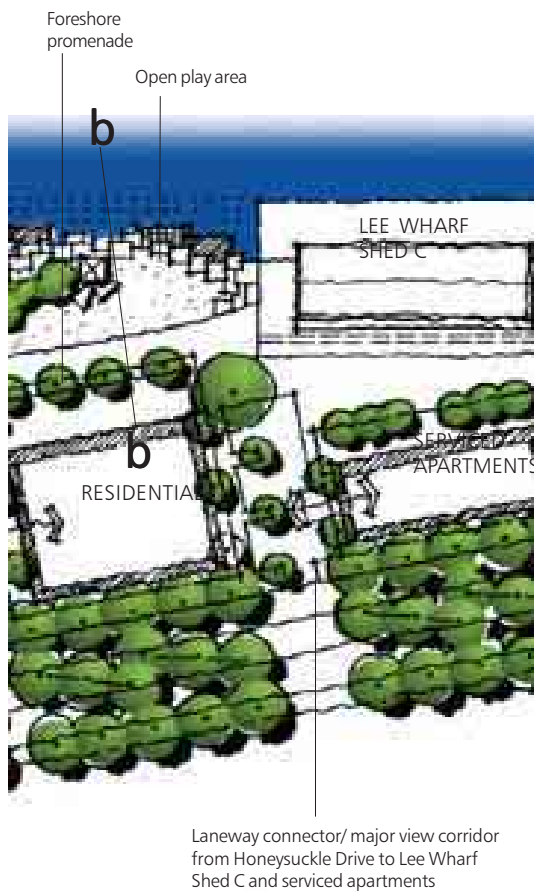


Location - Worth Place Park

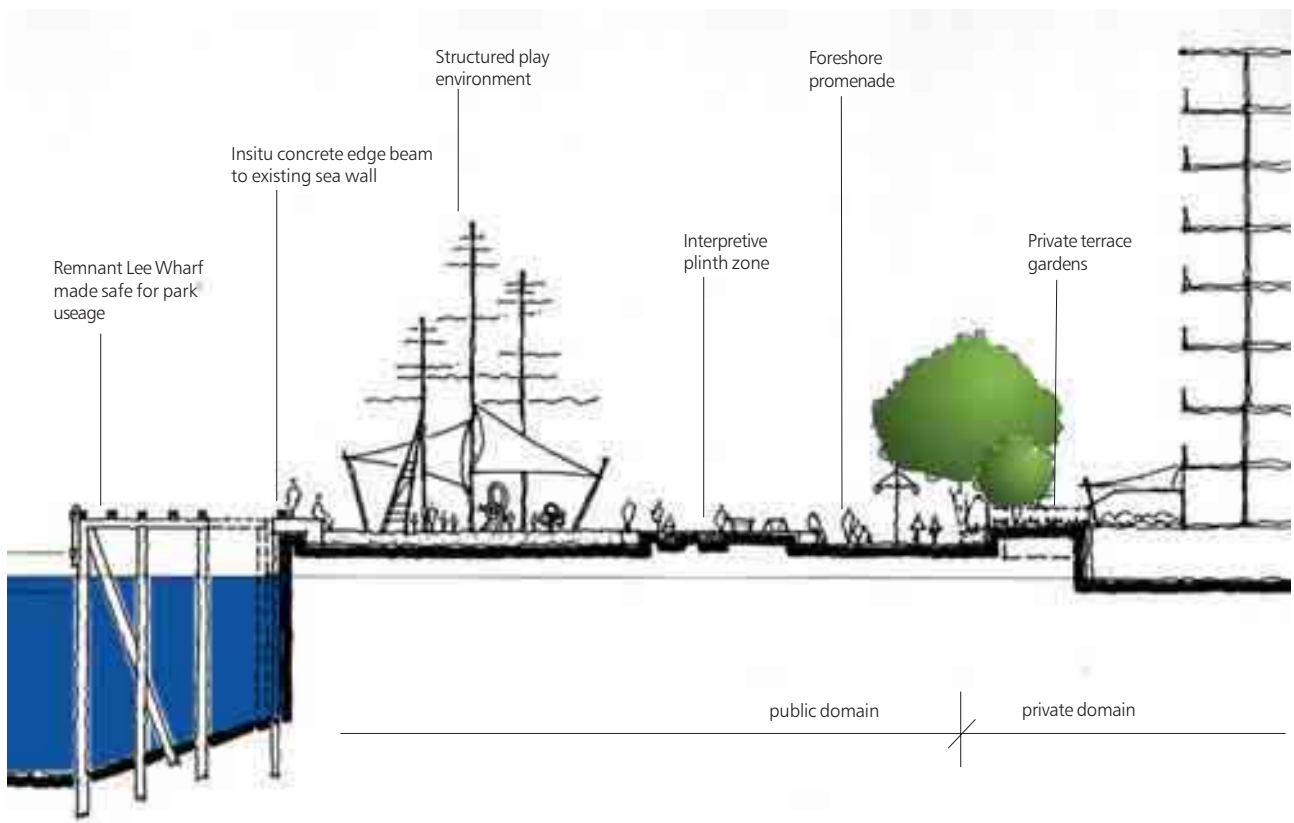
Objectives and Principles

The primary objective is to develop a highly stimulating, layered and accessible urban foreshore park that explores the idea of historical referencing, foreshore accessibility, ecological processes and contemporary play for all ages. The development of Worth Place Park is to consider the following urban design principles:

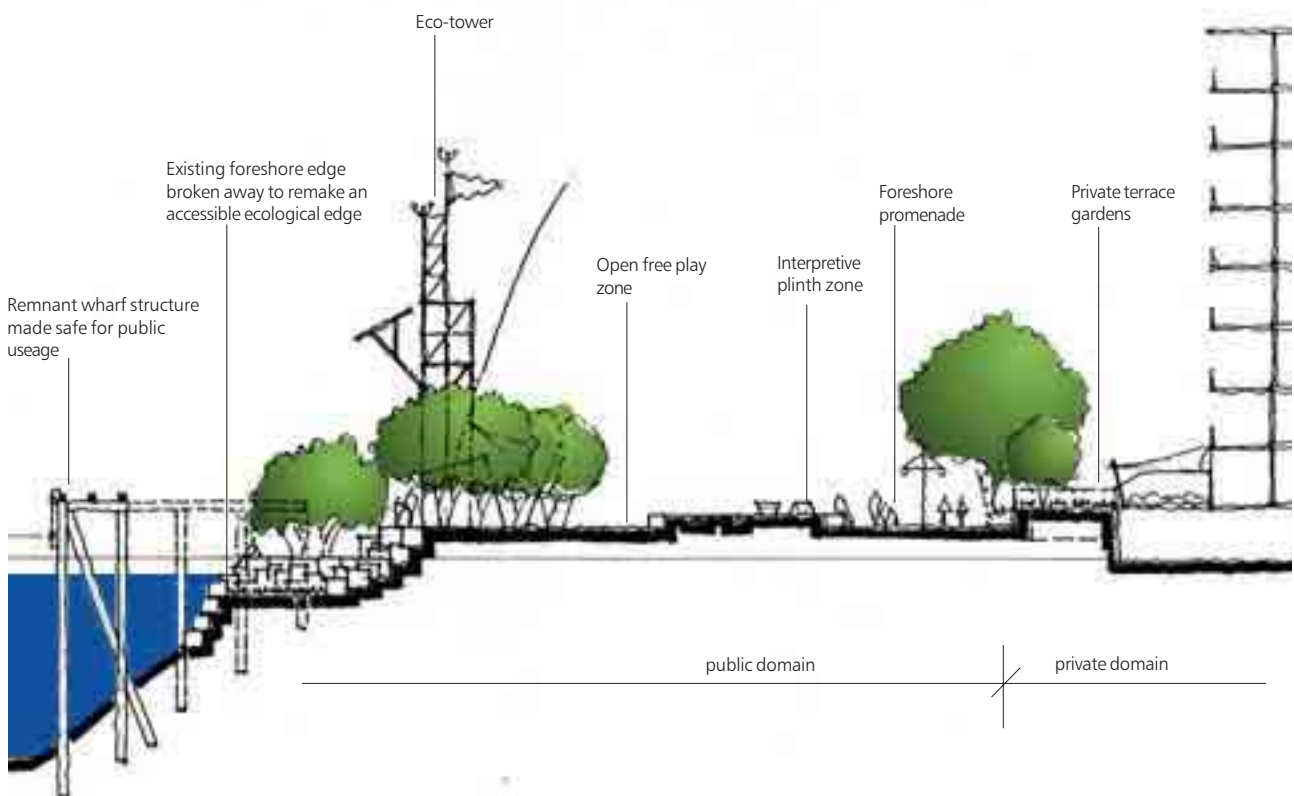
- Extend Worth Place into park as a linear intervention to accentuate the visual and physical gateway to the park and harbour beyond and to create a definitive fracture point between east and west;
- Incorporate on line stormwater management techniques to the the end of Worth Place connector including detention cells, macrophyte filtration and gross pollution controls prior to discharge into harbour;
- Create a linear east-west promenade element between Lee Wharf Shed A and HWC connector, with designated pedestrian and cycleway access to define the southern edge of the park and interface with private terrace gardens;
- Retain existing foreshore rail lines where possible and establish an appropriate curtilage to these industrial remnants in the form of retaining walls and pavement differentiation;
- Establish access to the harbour edge (to low tide limit) to experience natural processes such as tidal change and estuarine ecology and for recreational pursuits such as fishing as part of seawall rectification works;
- Retain and modify existing wharf structure as maritime industrial archaeological element (a symbol of change) whilst ensuring public safety with restricted access;
- Establish two distinctive park environments separated by the Worth Place connector that explores the dichotomy between urban (industrial) and natural processes, active and passive recreation, past and present;
- Ensure provision of adequate shade for variety of uses, exploring creative use of structure, fabric and vegetation;
- Develop creative play opportunities, both structured and non-structured, for all ages and user groups;
- Conform to and enhance DCP 40 view corridors to the harbour;
- Provide opportunities for regular stopping places for rest and refreshment along the foreshore journey; and
- Explore opportunities for incorporating natural elements (wind, sun, waves) into generation of green energy and artistic expression (sound, form, processes).



Refer following pages for typical sections



Typical section a.a. through western section of park



Typical section b.b. through eastern section of park

MATERIALS

Western Section

Edge Treatment

- Existing seawall repaired as required to ensure public safety.
- In situ concrete beams (minimum 2m - maximum 4m wide) to existing seawall at existing wharf level to allow foreshore access and function as seating element.
- Recycled wharf timber baulks minimum 300x300mm sections x 4000mm, dressed all round to remove splinters / obtrusions gang nailed at ends.

Structured Play Zone

- Turf to kickabout open lawn area and passive cells with shade structures.
- Softfall (sand, mulch, rubberised pavement) to active cells associated with mast structures.
- Sculptural mast elements with ancillary elements for shade, climbing, sliding, etc. (steel, timber, recycled plastic).

Pavilion

- Open sided 'cargo shed' with seating, tables, BBQ's, fish cleaning facilities.
- Terracotta roof tiles, timber posts and framing to compliment Lee Wharf shed typology.
- In situ concrete pavement.

Interpretive Zone Plinth

- In situ concrete walling and rampways.
- Remnant rail lines set in stabilised roadbase / crusher dust with 5% crushed oyster shell mix.
- Repetitive forms may include containers that reference coal wagons (corten/WR350 steel, chequerplate) in various stages of capacity (ie. full, empty, etc).
- Orthogonal mounds of raw material (ballast rocks, coal, crushed oyster shells, salt etc.), artistic interpretation of industrial materials stockpiling, etc.
- Ephemeral zone planting including macrophyte and native grassland / dune grasses.

Eastern Section

Edge Treatment

- Existing sea wall removed due to structural defects.
- Pre-cast concrete cubes of varying size (minimum 1x1x1m - maximum 3x3x3m) randomly stacked to define accessible foreshore edge and estuarine cells.
- Pre-cast concrete cubes set flush on grade to establish foreshore walkway areas.

Non structured play zone

- Turf to open lawn areas.
- Front line copsed vegetation including Banksia integrifolia, Leptospermum laevigatum etc. to spatially define open lawn and provide shade.

Activities

Worth Place Park should be designed to accommodate the following recreational activities:

- Foreshore access for pedestrians and cyclists;
- Historical interpretation and education;
- Structured play including climbing, sliding, swinging
- Non-structured recreation including rock hopping, kickabout, fishing; and
- Traditional passive activities including picnicing, BBQ's, group gatherings.

The potential to integrate a diverse range of activities and user groups in such a constrained linear space is to be carefully managed to guarantee public safety and accessibility. Future stakeholders should be considered, in particular residents in adjacent apartment developments (eg. issues of noise and public/private domain access) and tenants in Lee Wharf Shed (eg. Rowing Club and issues of access to water).

Spaces

Worth Place Park should comprise two primary spaces, the eastern and western sections defined and separated by two linear interventions, the east-west promenade and the north-south Worth Place connectors. Minor spaces including the HWC connector and other laneway connectors /view corridors would ensure the park becomes defined by built edge with permeable spaces.

The predominantly linear park will be spatially defined by a series of planar forms at varying levels determined primarily by the fracture line (Worth Place) between the section of seawall to be retained and the section requiring reconstruction.

The **Western (urban) section** of the park is to remain at the existing wharf level (existing seawall to be retained) and referencing past maritime industrial processes with elements such as sculptural mast structures associated with the structured play area, use of repetitive sculptural forms associated with retention of remnant rail lines, and cargo shed picnic pavilion.

The **Eastern (hybridised natural) section** of the Park is to be set down at a lower level as part of the seawall reconstruction works and referencing past ecological processes associated with the original tidal estuarine edge. The manipulation of the foreshore edge along seawall reconstruction creates a significant opportunity for creating a hybridised natural edge incorporating a series of successional cells of seagrass, saltmarsh, mangroves and freshwater macrophytes.

The incorporation of the remnant rail lines into a raised plinth will create an appropriate spatial connection between the two sections defining the linear intervention of the Worth Place connector.



Pavement rhythms



Example of playground equipment

MATERIALS (cont)

Eco Towers

- Designed to reference and promote use of green energy generation.
- Steel framing with marineply panels to base elements (toilets, etc).
- Solar panels, wind turbine.
- Shade fabric sales.
- Viewing platform.

Interpretive zone plinth

- As per western section.

Promenade

Pavement

- In situ concrete pavement (broom finish or exposed aggregate river pebble).
- 300x300 or 400x400mm honed pre-cast pavement as feature banding to promenade and threshold pavement to terrace gardens.
- Tactile pavers to terrace gardens threshold.

Delineators

- Pole top luminaires.

Worth Place Connector

Pavement

- Washed aggregate (blue metal) in situ concrete with permeable pre-cast pavement banding (Ecoloc or similar).
- Feature galvanised steel sections cutting through pavement banding to visually connect light structures.
- 150mm in situ concrete edge strip between footpath and connector pavements.
- Asphaltic concrete (AC10) to continuation of street verge footpath from Honeysuckle Drive.

Feature Light Elements

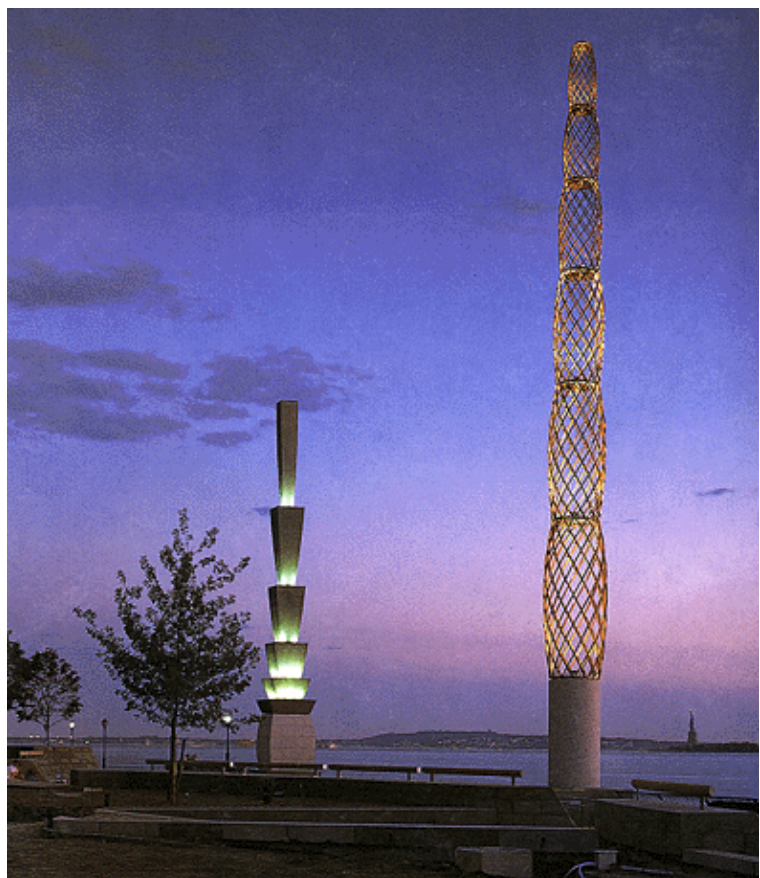
- Custom designed pole luminaires and vertical markers to define Worth Place view corridor
- Design to reference brownouts Art deco period, maritime industrial processes and tradition of celebratory arches.
- Material selection to incorporate steel as primary element with use of concrete or masonry as solid base to be explored.

Laneway Connectors / View Corridors

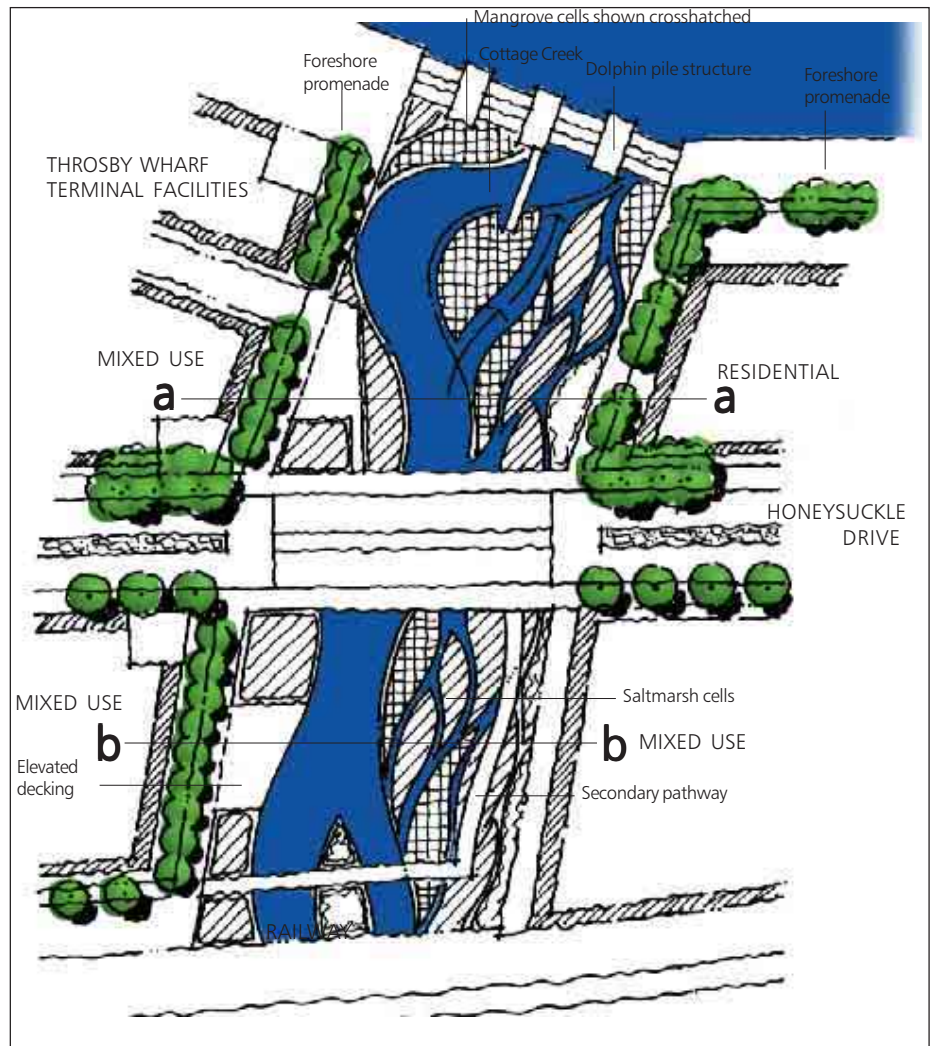
- Minor view corridors refer to materials section *Laneway Connectors / View Corridors*.

The Worth Place connector is to be a simple, clean lined space cutting through the linear park, ensuring sight lines to the harbour and Carrington Dockyards from Worth Place and Hunter Street. The view corridor is particularly important as one of the gateways to Honeysuckle and is to be defined by vertical luminaire elements, offset in a body of pavement that connects through to the foreshore from Honeysuckle Drive.

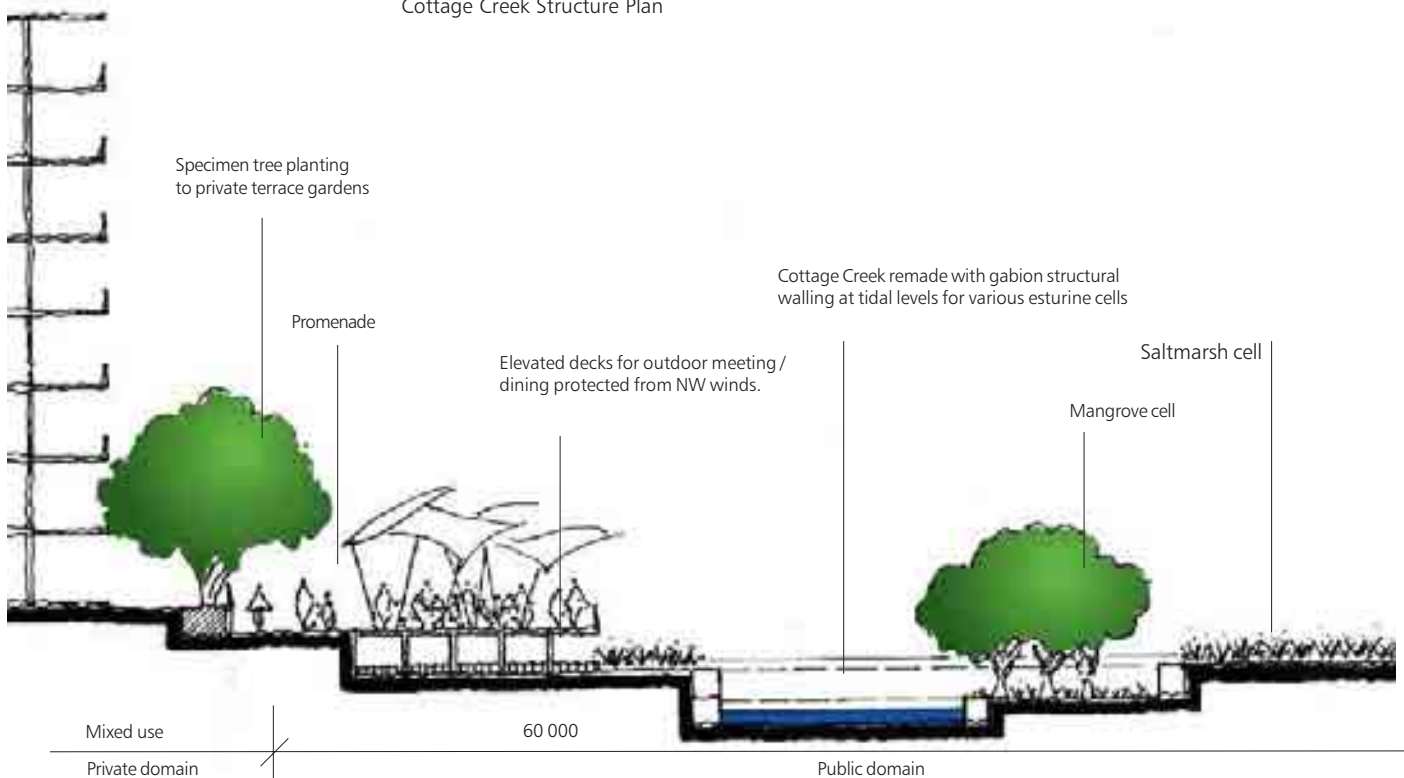
The foreshore promenade is to spatially define the southern edge of the park and establish a unifying linear element between the eastern and western sections. The ramping, arching form is to incorporate repetitive elements including specimen tree planting, lighting, seating and pavement definition.



Customised vertical luminaire elements such as Stainless Steel Pylons by Martin Puryear at Battery Park, New York would appropriately define the north place view corridor. Source: Beardsley, 1998

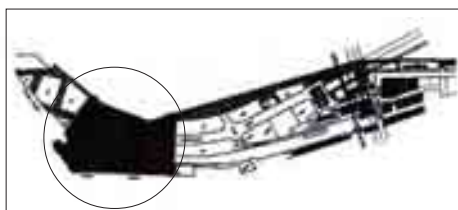


Cottage Creek Structure Plan



Typical section a.a. through Cottage Creek corridor

4.5 COTTAGE CREEK PRECINCT

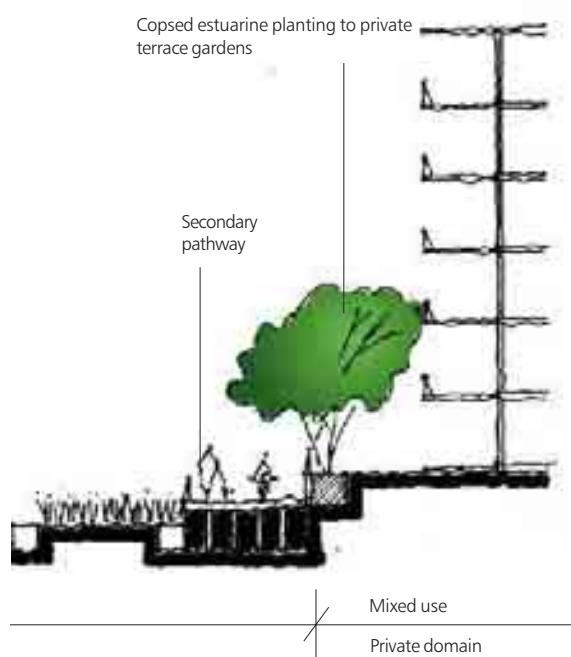


Location - Cottage Creek Precinct

MATERIALS

Dolphin Pile Structure

- Cottage Creek mouth crossing structure, connective element at transition zone on foreshore alignment;
- Designated pedestrian and cycleway bridges with transparent decking;
- Reference historic dolphin pile structures;
- Recycled hardwood timber pieces, headstocks and girders;
- Galvanised steel framing to decking and balustrade including use of perforated mesh panels to decking and decorative panels incorporating urban artwork as balustrade feature;
- Amenity lighting incorporated into structure; and
- Junction with existing Throsby Wharf structure to reveal raw profile of wharf i.e. sawcut concrete decking.



Vision Statement

Cottage Creek offers a unique opportunity to create a truly integrated urban waterway that transcends its current engineered form to become a living environment for both human and engineered systems. The remaking of Cottage Creek will create structural but fluid connections between the foreshore and the west end that offers a variety of spatial and ecological sequences in the move forward towards a sustainable city.

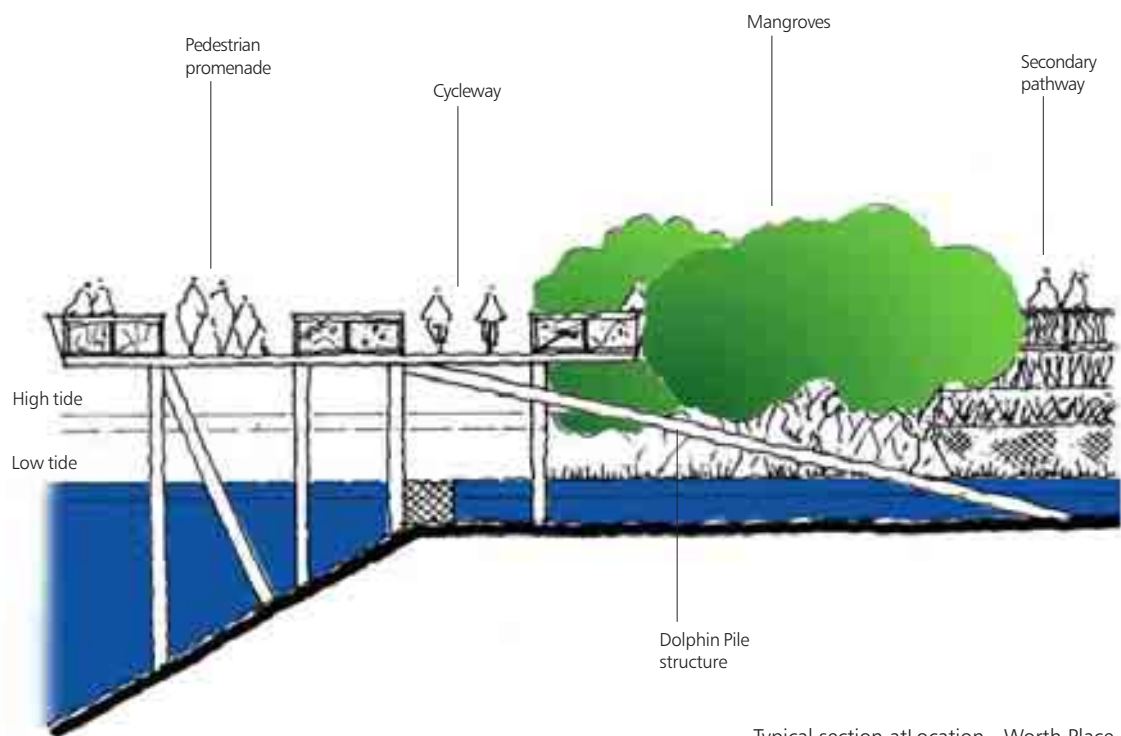
Objectives and Principles

The primary objective is to transform a highly degraded engineered stormwater canal into a hybridised urban waterway that balances ecological, flooding and water quality requirements against structural engineering potential cruise ship terminal operations at Throsby Wharf and recreational use of the new “edges” including pocket park and outdoor entertaining eating areas.

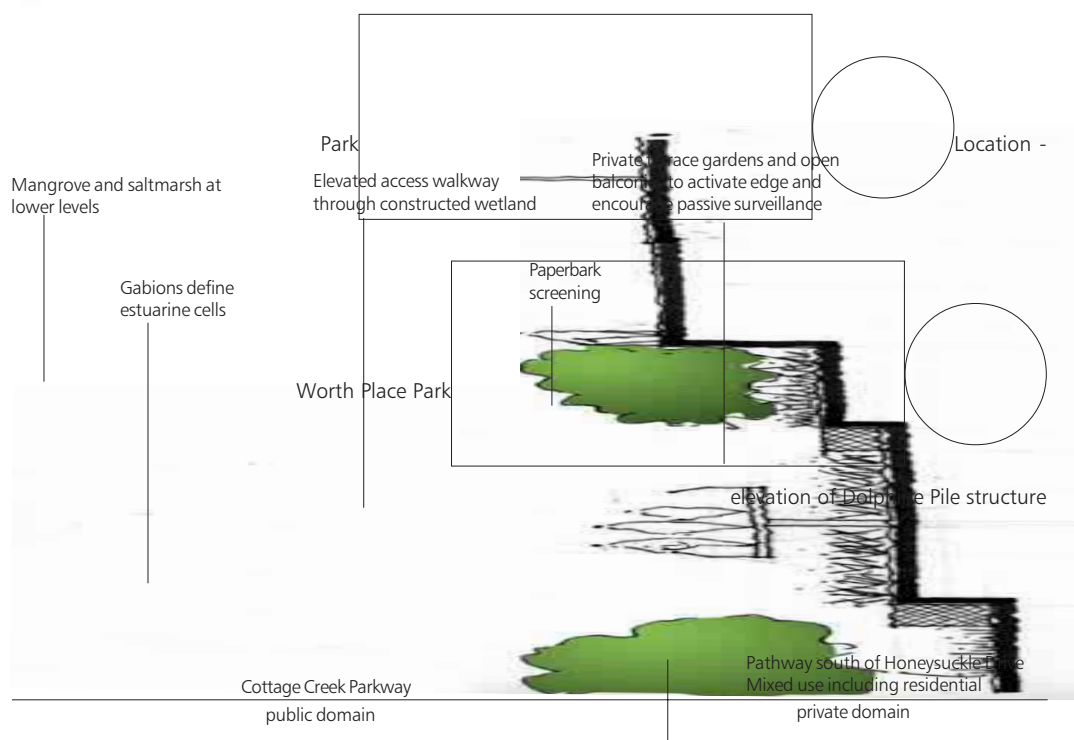
It is important to note that the objectives and principles outlined above are based on conceptual ideas developed for the purpose of this Strategy alone. Unlike the other precincts described previously, no detailed masterplanning work has been undertaken for the Cottage Creek Precinct in terms of use, development or investment interest. The conceptual ideas described in the Strategy are to form the basis for further detailed investigation as to what opportunities are truly feasible for this precinct. The preparation of a management plan that investigates and tests the economic, social and ecological feasibility and sustainability of these concepts is recommended.

The remaking of Cottage Creek should consider the following urban design principles:

- The development of viable and sustainable hybridised estuarine communities associated with the modified creek environment - seagrass, saltmarsh, mangroves and freshwater macrophyte communities are to be incorporated into engineered terraces;
- Reprofile engineered channel consistent with principles of integrated stormwater management including gross pollution removed, water polishing, stormflow retention/detention, and streambank dynamics;
- Ensure equitable, permeable access along and across creek easement including across Honeysuckle Drive;
- Develop a series of spatial experiences along the creek that maximise sunlight, offer protection from prevailing winds, relate to adjacent developments at ground level and maximise access/contact with the creek environment;
- Development of adjacent properties to integrate with the creek environment including activated ground floor uses, mixed uses above with accessible and useable outdoor spaces to encourage overlooking and adequate passive surveillance;



Typical section at Location - Worth Place



Typical section b. b. through secondary pathway south of Honeysuckle Drive

MATERIALS (cont)

Pedestrian Access

Primary pathway and meeting places

- In situ concrete pavement with use of oxides and exposed aggregates to be incorporated as appropriate;
- In situ concrete deck areas with concrete piles, galvanised steel handrails, shade structures and associated seating/urban furnishings, lighting;
- Pole mounted luminaries to delineate pedestrian/cycleway access;

Secondary pathway

- In situ concrete with galvanised steel handrails north of Honeysuckle Drive;
- Lightweight steel walkway with perforated mesh decking and galvanised steel handrails south of Honeysuckle Drive; and
- Pole mounted luminaries to edge of pathways - no delineation of pedestrian/cycleway required.

Creekline Structure

Walling to edge of easement

- Retention of existing concrete walling/wharf structure where possible; and
- New walling to be concrete gravity walling with recycled steel timber and steel piles or similar.

Walling within creekline

- Basalt rock filled gabions to define estuarine cells.

- Ensure appropriate amenity lighting for pedestrian security and feature lighting of elements such as bridges and dolphin pile structure;
- Create a highly designed and structured creek environment that reflects its urban context yet demonstrates a clear understanding of the natural processes (estuarine and urban stormflow) that determines the underlying structure;
- Incorporate an appropriate dialogue that references the past including the graveyards and Bellevue Hotel;
- Ensure appropriate setbacks on buildings adjacent to creek to maximise sunlight access into corridor; and
- Create a positive, functional connection through to the harbour at the remade mouth of the creek.

Activities

Cottage Creek offers an opportunity to become a unique urban meeting place for people and natural processes. Proposed activities will comprise both cultural and hybridised natural activities and processes.

- Cultural activities include:
 - Pedestrian/cycle connection from Hunter Street through to foreshore;
 - Resting, refreshing, dining and gathering along a series of edge spaces associated with activated ground floor users;
 - Interpreting, engaging and exploring the remade creek environment; and
 - Overlooking from balconies associated with naturally ventilated offices, apartments.
- Continuous foreshore pedestrian cycle connection via the Dolphin pile structure as called for in DCP 40. (This connection is particularly important if Linwood and Wickham are to remain connected to the Honeysuckle and Civic Precincts.)
- Hybridised Natural Processes
 - Stormwater and flood management;
 - Estuarine communities established for habitat, interpretive and water quality functions; and
 - Landscape aesthetic determinants.

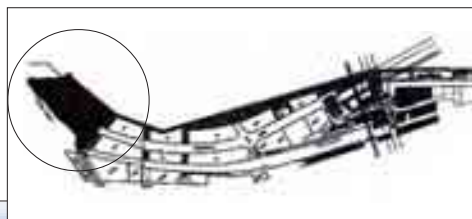
Spaces

Cottage Creek will comprise a series of north-south linear spaces defined by east-west connectors (foreshore edge, Honeysuckle Drive, Railway Corridor and Hunter Street). These east-west transects offer opportunities for highly active interaction points along the linear sequence and are designed as controlled pause points. The progression of spaces involves a sequential transition from the highly structured natural systems at the foreshore edge through to a more urbanised creek environment at Hunter Street. Likewise the cross sectional creek line transect is to comprise a highly structured edge environment on the western side to a more hybridised naturalistic treatment to the eastern side. Generous paved platforms for congregational activities are to be incorporated as part of this structured western edge.



Wickham Village Structure plan

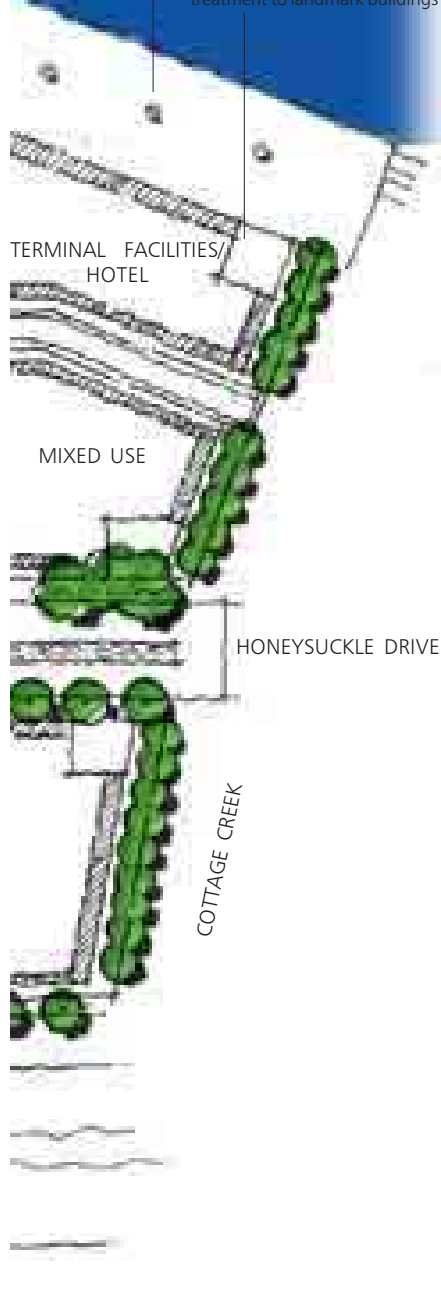
4.6 WICKHAM URBAN VILLAGE



Location - Wickham Urban Village

Ionic vertical elements as signature arrival statement to Throsby Wharf Terminal

Opportunities for articulated corner treatment to landmark buildings



Vision Statement

Wickham Urban Village forms an important nexus between the Cottage Creek environs, City West, Wickham residential area and the proposed cruise ship terminal and marina precincts. The opportunity to build on the ideas developed in the Wickham Urban Village study is to be grasped with the development of a richly layered, fine grained urban precinct that forms the western gateway to Honeysuckle.

Activities

The rich layering of diverse urban activities will distinguish Wickham as a true urban village area within Newcastle. These may include:

- Mixed housing types including affordable housing to create a diverse resident base and shops top housing/SOHO housing to cater for integrated work/living environments;
- Village retail to service local population;
- Mixed employment infrastructure including hi-tech, arts, media and communication, clean industry, studios, which are often associated with urban villages such as Greenwich Village, N.Y.;
- Cruise ship terminal and ancillary infrastructure such as hotel, serviced apartments, transport interchange;
- Village based recreation, including cafe strips, urban pocket parkland, markets; and
- Commuter based activities associated with station including people arriving from outside and people leaving from within.

Objectives and Principles

The primary objective is to create a distinctive urban village precinct that responds to the existing urban morphology and typology of Wickham residential area, building on the existing infrastructure of Wickham train station, Cottage Creek, Throsby Wharf and City West retail core, and to establish opportunities for new forms of integrated working, living and recreation

The area of land referred to as Wickham Urban Village is not a currently under the authority of HDC, but was considered an important piece of the whole, an integral part of the Honeysuckle redevelopment program. Therefore it is important to note that the objectives and principles outlined below are based on conceptual ideas developed for the purposes of this strategy alone.

The creation of an urban village at Wickham should consider the following urban design principles:

- Adoption and development of ideas, principles and concepts outlined in the Wickham Urban Village Study (Honeysuckle and Environs Area Committee, Building Better Cities Program);
- Development of the 'Town Square' associated with the train station and old School of Arts building;



Navigational towers

MATERIALS

Lawson Square

Pavement

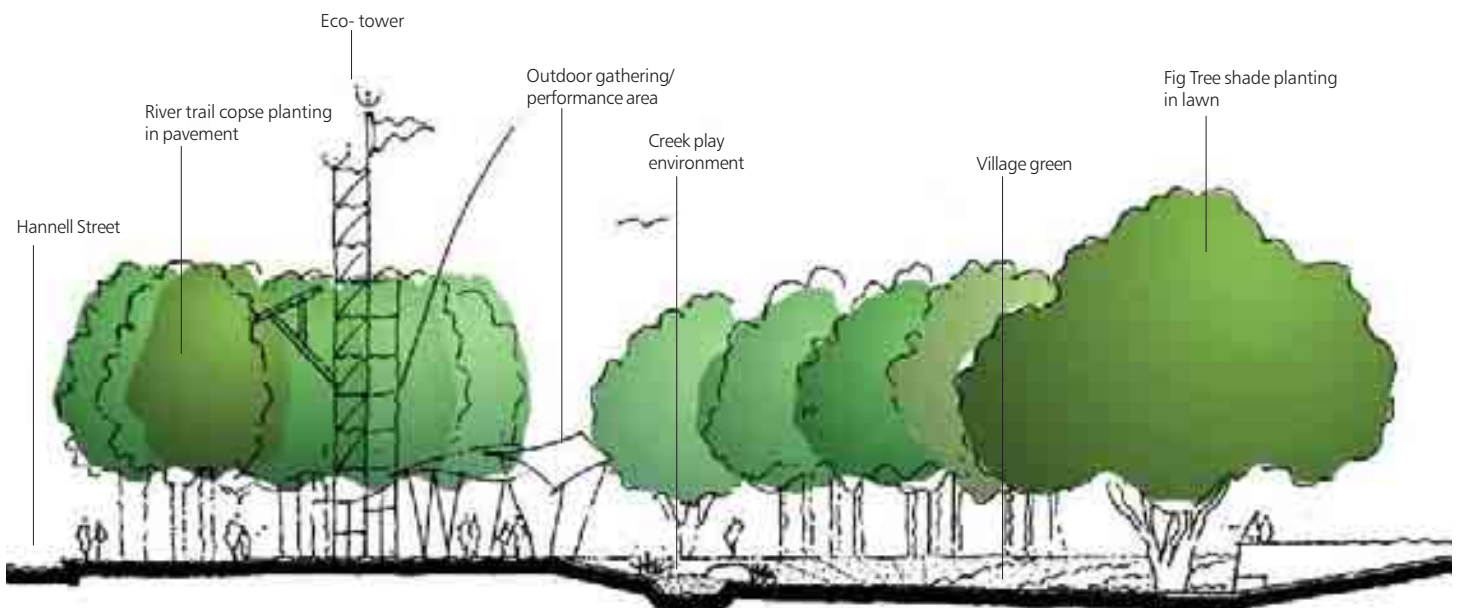
- Oxidised insitu concrete banding on old Wickham grid alignment;
- Feature element e.g. galvanised steel sections to create alternate banding on new Wickham grid alignment;
- Alignment of pavement interface with open lawn area to reference natural alignment Cottage Creek prior to channalisation; and
- Copsed tree planting associated with river trail metaphor.

Village Green

- Open lawn area addressing Wickham Station;
- Level change to create sloping plane toward north-east corner of park to reference flow of creek to harbour; and
- Textural definition of creekline e.g. sand with placed rock and planting associated with opportunities for integrated play elements.



Vertical iconic elements are a feature of Newcastle Harbour



Typical section through Lawson Square

MATERIALS (cont)

Structure

- Opportunities for incorporating Eco-Tower with associated shade structures as iconic landmark feature within park including toilet;
- Information and performance facilities; and
- Walled plinth area with paved podium to eastern area to overlook village green with potential for BBQ's, seating, play and shade elements.

Waterfront Place

Pavement

- Existing concrete deck to wharf, water blasted to renew surface as necessary;
- Retention of wharf elements such as rail lines; and
- Investigate opportunity to incorporate sandblasted sections as feature detailing possibly associated with iconic elements.

Edge Treatment

- Retention of existing wharf edge including fenders; and
- Modified to accommodate operational requirements of proposed terminal.

Structure

- Iconic vertical elements to reference existing shipping communication elements and create a strong sense of arrival to Newcastle;
- Galvanised steel structure/framing;
- Opportunity for incorporating solar power and wind generation elements;
- Fabric banners, sail elements and wind sculptures; and
- Contrasting colour panels to reference orange panels used for shipping communications.

- Development of permeable, legible streetscape arrangement defined by fine grained, street edge built form;
- Consider opportunities for retaining and retrofitting Throsby Wharf used as part of the cruise ship terminal;
- Develop appropriate contextual built edges to relate to adjacent precincts;
- Introduce built form to intersection of Stewart Avenue and Honeysuckle Drive to create 'gateway' to Honeysuckle; and
- Promote high quality design outcomes focusing on new forms and typologies for integrated working, living and recreational environments that are definitive of Newcastle in the new millennium.
- Mixed employment infrastructure including hi-tech, arts, media and communication, clean industry, studios, which

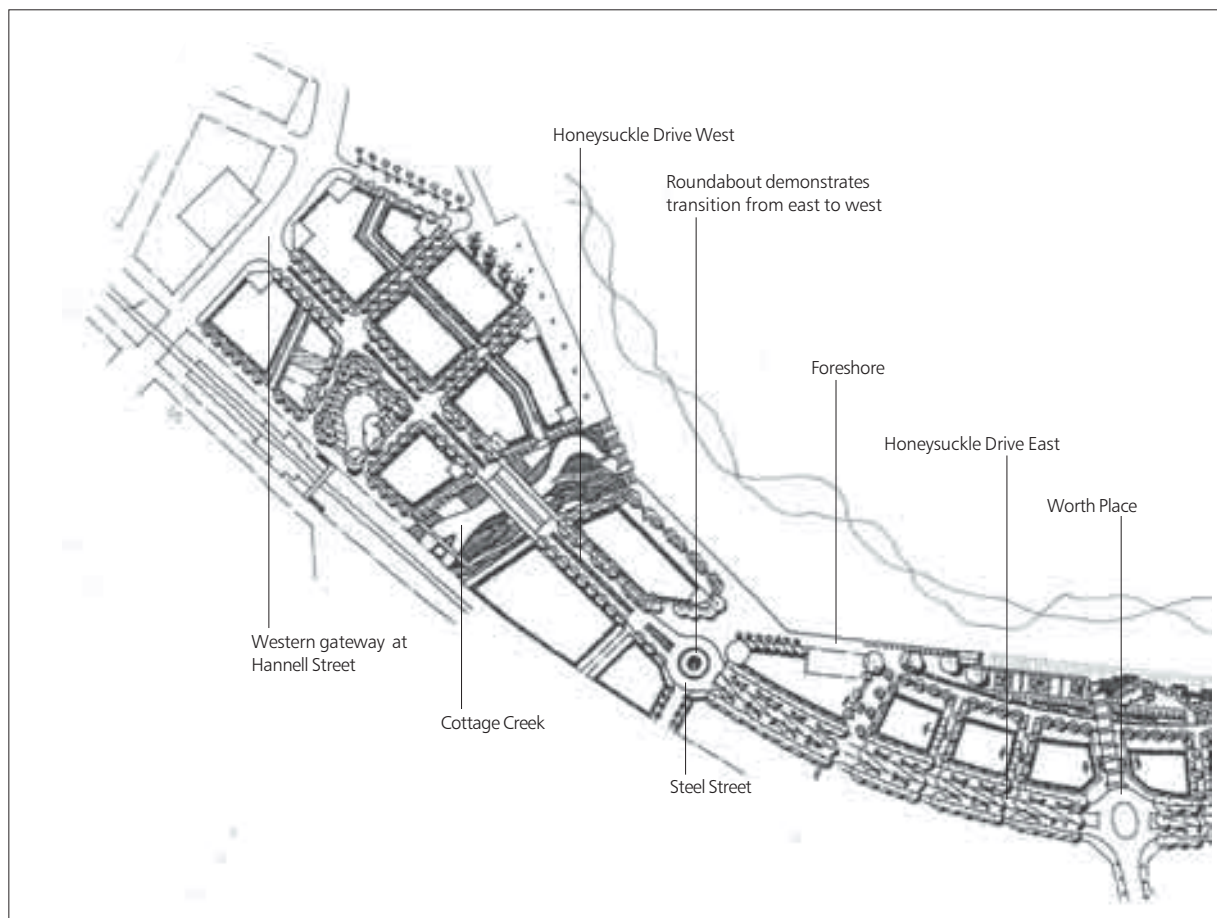
Spaces

Wickham Urban Village is spatially defined by the rail easement, Cottage Creek, foreshore and Hannell Street arterial road. These active edges create opportunities to ensure the village is a permeable, accessible and a inclusive place comprised of a series of streetscape spaces, urban pocket parks, Cottage Creek interface and foreshore connections.

The streetscape patterning and legibility is key to the successful functioning and imageability of Wickham as an urban village. A distinctive hierarchy of primary roads (Honeysuckle Drive, Hannell Street), access roads (including view corridors) and laneway connectors creates a series of interconnected streetscape spaces that establishes clear east-west and north-south movement pathways. The existing on-grade rail crossing at Wickham Station and proposed Hannell Street crossing will ensure accessible connections to City West and vice-versa. Connections through to the foreshore are to be maximised by permeable streetscape and open space connections.

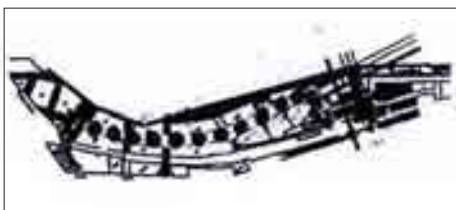
The Waterfront Place will be largely determined by whether or not the proposed cruise ship terminal occupies Throsby Wharf. The strategy recognises that should a terminal be developed at Throsby Wharf then a simple open foreshore space be established to allow for the operational logistics of docking large cruise ships. The space should be designed to accommodate public access during operational mode. A strong sense of arrival should be established at the waterfront place with a series of vertical, iconic elements designed to reference harbour communications and docking processes.

The urban pocket park, Lawson Square, is to be the central spatial focus of the village. The park is to function as an urban square with peripheral areas of pavement sufficient for markets and gatherings. Whilst an open grass area suitable for a village green/kickabout area is to occupy the central area of the park. The park is to be a fusion of metaphor and traditional park paradigm. The river trail is to reference the alignment of Cottage Creek prior to its natural channelisation, through considered delineation of hard and soft space, pavement patterning and selected riparian tree planting.



Overall structure plan for Honeysuckle Drive

4.7 HONEYSUCKLE DRIVE



Location - Honeysuckle Drive

Vision Statement

Honeysuckle Drive will become one of the premier boulevards in Newcastle, much along the lines of Hunter and King Streets. This new boulevard will be contextually and ecologically integrated to create a highly layered yet legible streetscape experience.

Activities

The primary activity or function of Honeysuckle Drive is an east-west conduit for both internal and external circulation of vehicular, cyclist and pedestrian traffic. In the tradition of great streets, the boulevard should also encourage activated street edges at ground level, in particular at intersection points with north-south connectors. The relationship of ground floor living areas and apartment pedestrian egresses are to be designed to encourage occasional street level interaction between residents and by-passers.

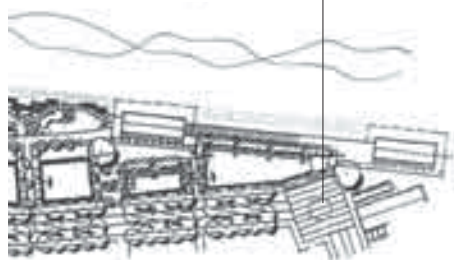
Objectives and Principles

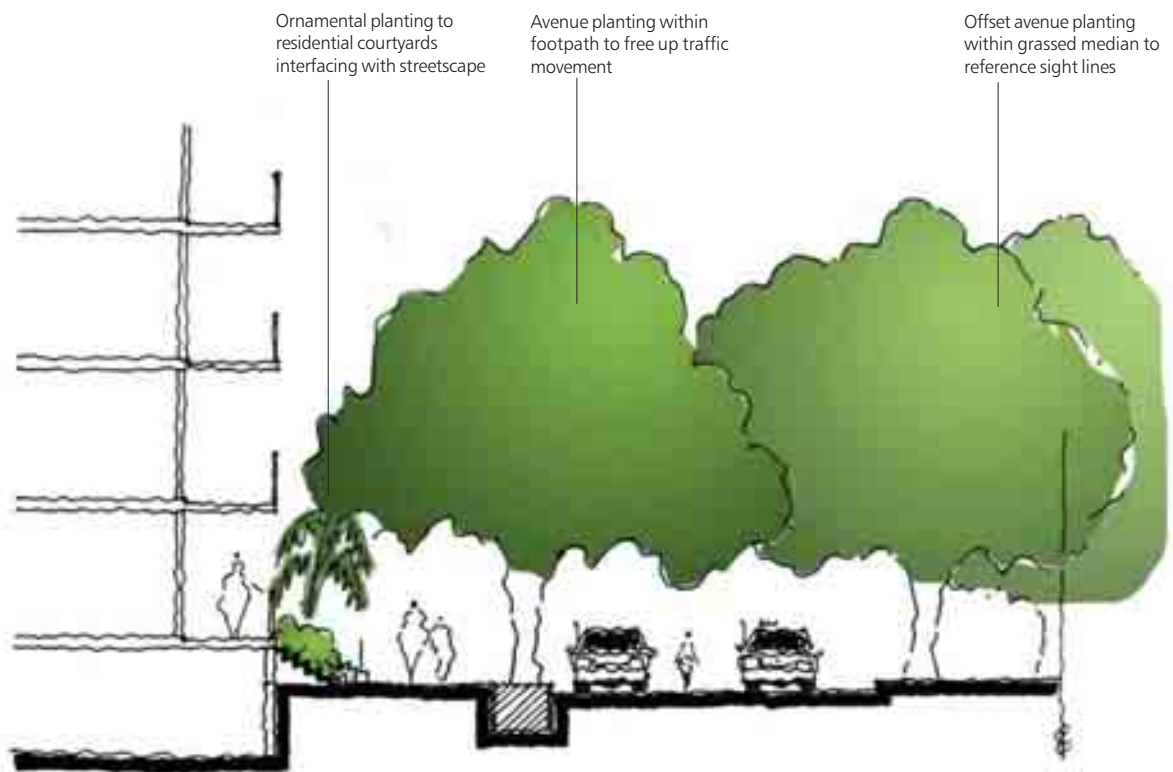
The primary objective is to establish Honeysuckle Drive as a memorable boulevard experience providing the primary east-west linkage through Honeysuckle and engaging with the north-south connectors/view corridors in a deliberate and considered manner.

The (re)-development of Honeysuckle Drive should consider the following urban design principles:

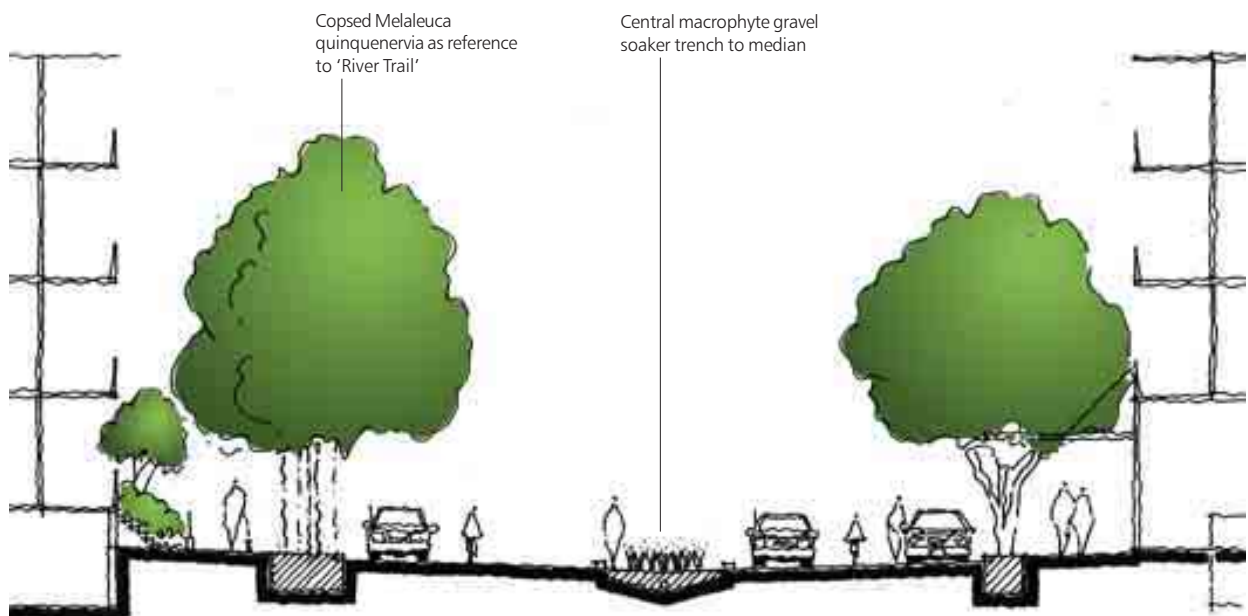
- Establish two distinct streetscape characters that correspond to specific contextual and ecological situations, namely the eastern section between the squareabout and Steel Street roundabout and the western section between the Steel Street roundabout through to Stewart Avenue;
- Establish appropriate eastern and western termination points to the Boulevard;
- Establish meaningful and engaging intersection points between the Boulevard and north-south connectors, e.g. Worth Place, Steel Street, HWC Connection, Cottage Creek
- Incorporate design metaphor of river trail concept into Boulevard Street tree planting in the western section.
- Develop a pallet of streetscape elements that is contextually relevant to Newcastle city streets, in particular other Boulevards/main streets such as King and Hunter Streets; and
- Integrate sustainable stormwater management principles to the western section including on grade soaker trenches connecting to Cottage Creek.

Eastern termination at
Harbour Leisure Centre





Typical section through Honeysuckle Drive (eastern section)



Typical section through Honeysuckle drive (western section)

MATERIALS

Pavements

- Asphaltic concrete (AC10) with AC3 wearingcourse to footpaths to create contextual relationship with traditional Newcastle streetscape;
- Optional feature header course / banding with 300x300mm or 400x400mm pre-cast concrete unit pavers;
- Asphaltic concrete (AC10); and
- Line marking and/or coloured bitumen to delineate cycle lane.

Kerbing

- Insitu concrete kerbing @ 200mm wide or to match into existing dimensions;
- Insitu concrete pram ramps to intersections and at designated materials between corners; and
- Insitu concrete flush edge strip with pre-cast concrete wheel stops to prevent vehicular access and allow pedestrian/stormwater flow into median soaker.

Furnishings

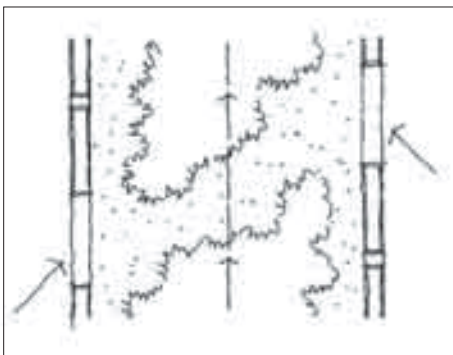
- Street lighting to match existing; and
- Seating, bins and bollards as per Honeysuckle Street furniture family to future selection.

Median treatments

- Eastern Section: Retain existing lawn.
- Western Section: No fines concrete to edges and walkways with 10mm blue metal gravel mulch to macrophyte planting.

Cottage Creek Bridge

- Should funds permit a new creek crossing to be constructed over Cottage Creek, the bridge shall be designed as two independent arrival sections with a central void to allow sunlight into the creek environs; and
- Off-form concrete bridge spans and balustrading with asphaltic concrete road surface and concrete footpath.



Plan of central drainage system to median

Spaces

Honeysuckle Drive is to comprise two primary character zones, the eastern and western sections, and a series of secondary spaces at intersection points.

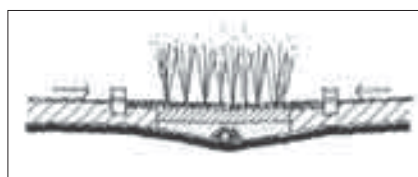
Whilst the **eastern section** has only recently been completed and implementation funds have been spent, some amalgamation in the future would be beneficial as part of a maintenance or management program.

- Attention to soil/nutrient relationships in an attempt to promote stronger tree growth in the depleted sandy soils, alternative tree selection maybe required in the long term.
- Avenue planting within footpath;
- Offset avenue planting alignment to median strip to reference sight lines of vehicular traffic;
- Establish avenue planting in equal groups determined by spacing of view corridors/laneway connectors; and

The **western section** will comprise the planned extension of Honeysuckle Drive from the proposed Steel Street connector through to the realigned Hannell Street. The proposed road reserve should be slightly narrower than the eastern section limiting the opportunity for tree planting to the median. Instead, given the relationship of Honeysuckle Drive to Cottage Creek the streetscape should incorporate ecologically sustainable stormwater management principles. These principles involve water harvesting, detention and filtration prior to discharge into the creek.

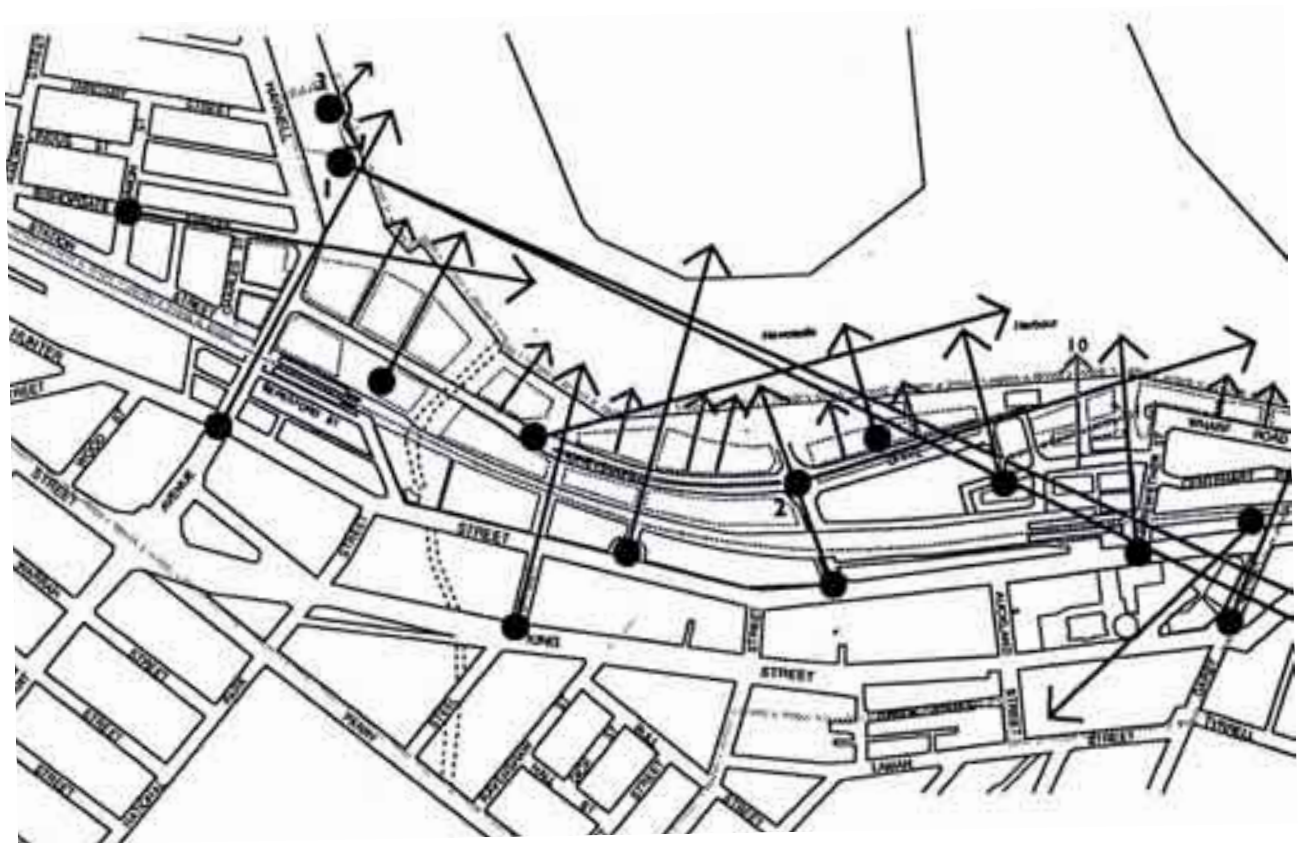
A central macrophyte gravel soaker trench to the median collects stormwater runoff from the roadway, reduces stormflow velocity and allows filtration prior to discharge. This will create a more textural, open streetscape in comparison to the canopied streamlined streetscape to the east. The use of consistent avenue tree species will ensure a coherent canopy treatment along the entirety of the boulevard.

The use of estuarine species associated with the river trail metaphor will create an appropriate contextual constraint associated with Cottage Creek.



Section through central drainage to

- ← 15m view corridors + public access
- 20m & major view corridors + public access
- Panoramic view corridor
- 10 ← 10m view corridors + public access
- City West Boundary



City West Development Area: View Corridors Source: City West DCP 40

Vision Statement

The north-south connectors and view corridors are integral physical and visual linkages between city west (Hunter Street) and the foreshore. They offer the opportunity to create contextually meaningful transects across the east-west linear morphology of Honeysuckle and ensure critical connectivity back to the city.

Activities

The primary activity and function of the connectors is their north-south linkages between the foreshore and Hunter Street. The connectors are intended to be highly activated linear spaces with ground floor retail and commercial uses as an extension of Hunter Street retail precinct.

The proposed ground floor activities should be facilitated to encourage a high degree of pedestrians activity and movement to ensure these spaces develop as pedestrian priority zones. Uses include outdoor dining areas, display areas, window displays etc.

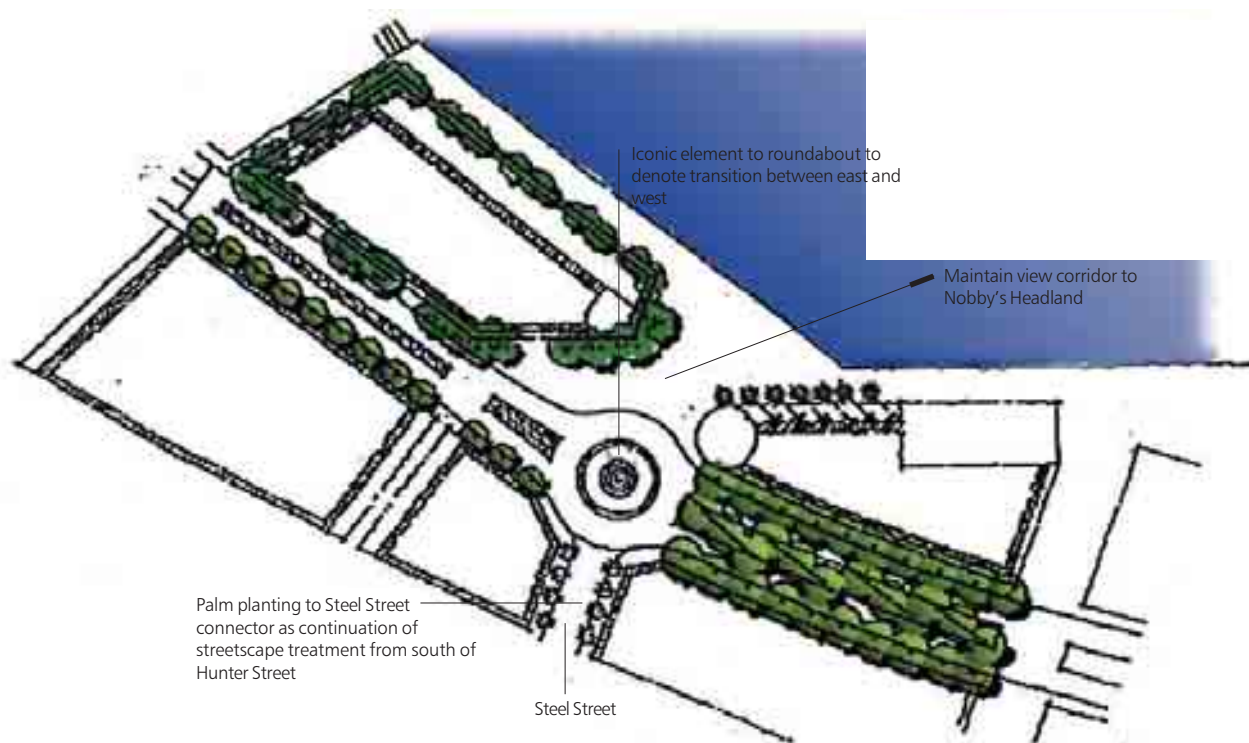
Objectives and Principles

The primary objective is to create highly accessible, activated and interconnected linear spaces between Hunter Street and the foreshore.

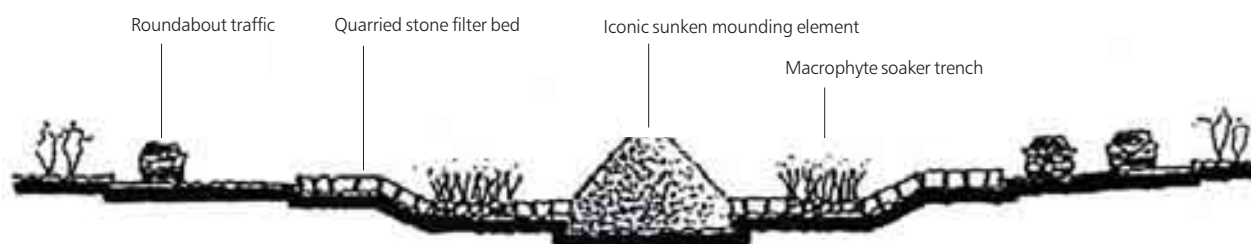
The development of north-south connectors/view corridors forms an integral part of DCP40 and should consider the following urban design principles:

- Linear spaces are to have an intrinsic coherent spatial character that references contextual, historical and ecological processes as appropriate;
- Linear spaces are to have a high degree of legibility and imageability and become memorable places in there own right;
- Establish activated street edges, in particular at intersection points with east-west conduits such as the foreshore, Honeysuckle Drive, Railway Corridor and Hunter Street;
- Identify opportunities to extend connections beyond Hunter Street, in particular with Cottage Creek and Steel Street through to the National Park;
- Establish pedestrian priority where possible, in particular within the laneway connectors and foreshore view corridors;
- Investigate opportunities to include urban artist input into treatment of key intersection points such as roundabouts, road and rail crossings and Cottage Creek Bridge;
- Establish highly legible and safe pedestrian egress across east-west connectors namely Honeysuckle Drive and the Railway corridor; and





Steel Street Connector



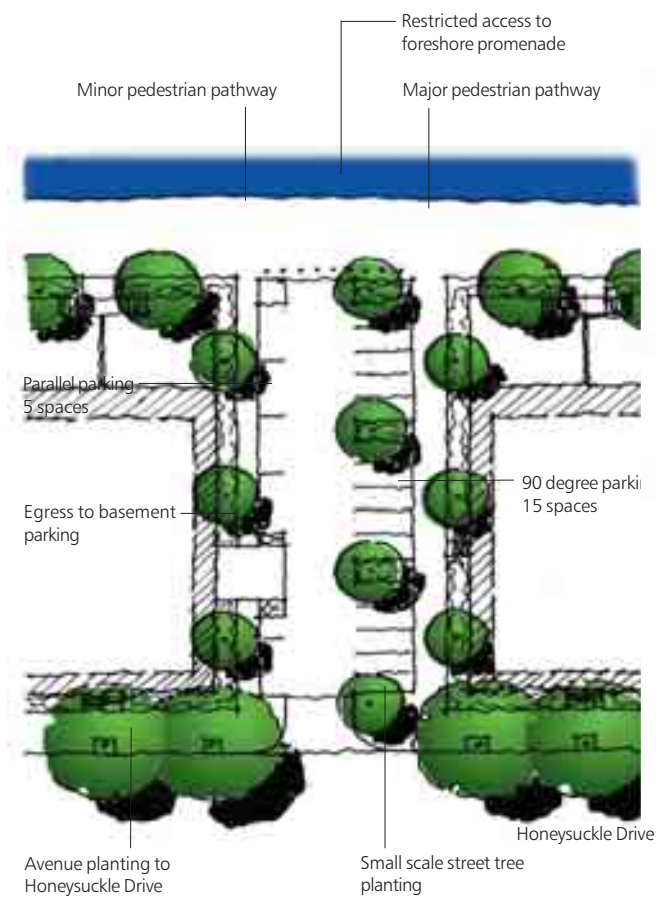
Section through Steel Street Connector

- Investigate opportunity of connection between foreshore through to the Technical college and/or Post Office on Hunter Street connecting through to King Street via the pedestrianised Devonshire Street.

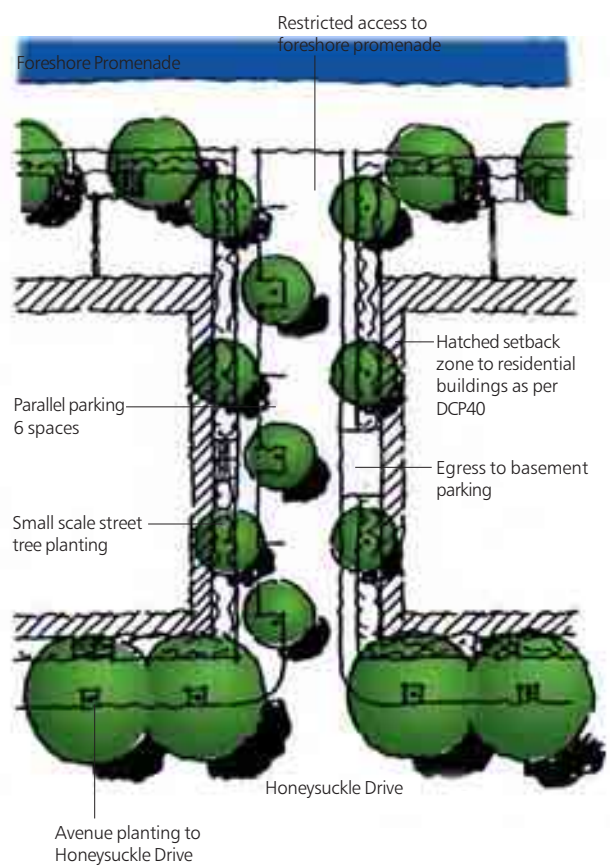
Spaces

The north-south connectors are comprised of primary linear spaces and numerous minor laneway/view corridor spaces. The primary spaces include:

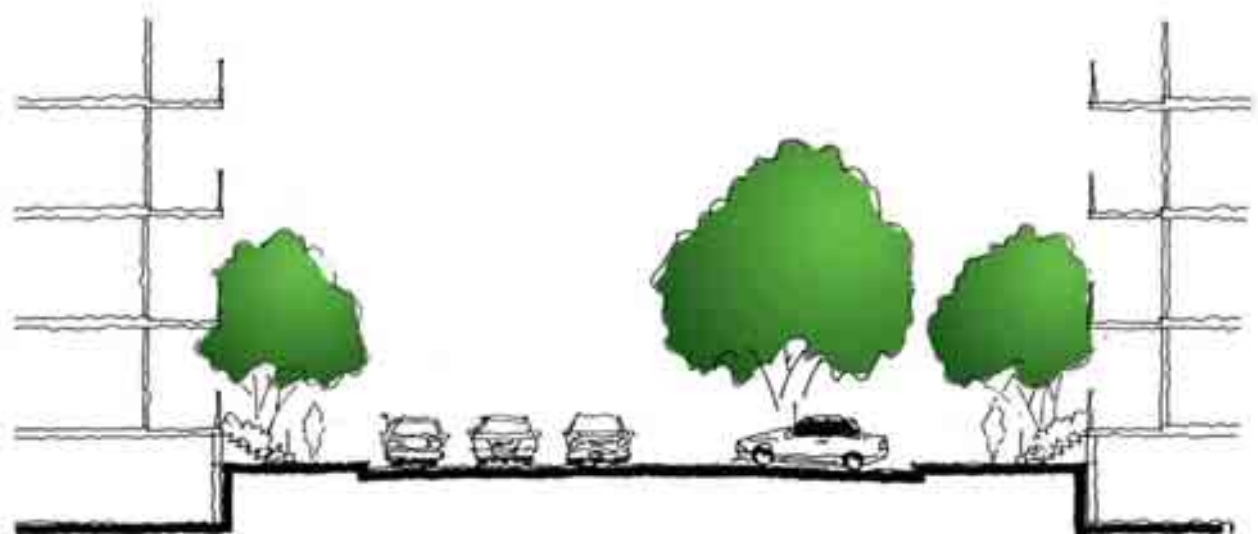
- Worth Place connector;
 - Hunter Water Corporation (HWC) view corridor;
 - Steel Street connector;
 - Cottage Creek/Hannell Street linkage; and
 - Stewart Avenue.
- **The Worth Place Connector** through to Hunter Street will create a significant gateway opportunity - both in terms of the most direct physical access into Honeysuckle and as an historical gateway that references past from the west end linkages to Carrington via the Bullock Island Bridge. The gateway is to be a simple yet highly legible space that incorporates repetitive vertical luminaire elements to create portal elements that frame views (day and night) to the harbour and industrial landscape beyond. Another opportunity could be to incorporate cinematic forms using projected light in conjunction with the Illumination Newcastle Project. No trees are to be planted along Worth Place and removal of the existing toilet block is obviously recommended.
 - **The Hunter Water Corporation (HWC) Connector** is unique in that it also acts as a reverse view corridor - focusing both on the harbor and the heritage HWC building. The HWC connector is an important pedestrian linkage from the western end of Worth Place Park through to Hunter Street and onto King Street via an existing pedestrianised laneway. An opportunity exists to create an activated pedestrian linkage with outdoor dining and smaller boutique retail and commercial uses, in particular associated with the mixed use residential foreshore development to the immediate west of Worth Place Park and the proposed square directly opposite the HWC building.
 - **The Steel Street Connector** is another vehicular gateway opportunity particularly given the proximity of the Honeysuckle Drive roundabout to the foreshore and its significance as denoting the transition between the eastern and western sections of Honeysuckle Drive. The roundabout offers a significant opportunity for an interpretive urban art installation such as iconic sunken mounding element (infering coal, oysters) surrounded by a tidal moat. It is important that the roundabout engage with both Honeysuckle Drive and the adjacent Steel Street Square. The foreshore square sits at the junction of the primary foreshore alignments - Lee Wharf and Throsby Wharf. Sight lines to the harbour and along the promenade are to be maximised



Typical plan layout of major (20m) view corridor



Typical plan layout of minor (15m) view corridor



Typical section through major view corridor

with an open plaza space strongly defined by urban plantings as a plinth to expressing the built form of the adjacent buildings. The continuation of palm plantings from Steel Street south is recommended to create a legible connection back through to the National Park.

- **The Cottage Creek/Hannell Street Connector** offers the opportunity for a highly permeable pedestrian circulation network through Wickham Urban Village - a prerequisite for urban villages. The development of the river trail metaphor from Hannell Street south, through Lawson Square along Honeysuckle Drive to Steel Street Square is the primary connective thread from Hunter Street to the foreshore. The use of riparian plant material, planted in copses rather than at regular spacings, is to be the primary referencing element tracing the historic Cottage Creek alignment. The development of Lawson square and commercial sites adjacent to the Bellevue Hotel are to incorporate the river trail memory line as an integral public domain element.
- The connection of **Stewart Avenue** through to Hannell Street north will create a new northern gateway into the Newcastle CBD as well as the western gateway to Honeysuckle. Stewart Avenue north will create another important foreshore connection to Hunter Street and view corridor through to the proposed marina complex. The arterial road priority will ensure that this is an intensive vehicular space, particularly at the Hunter Street and Honeysuckle Drive intersections. Adequate provision for pedestrian movement and safety of crossing is imperative.

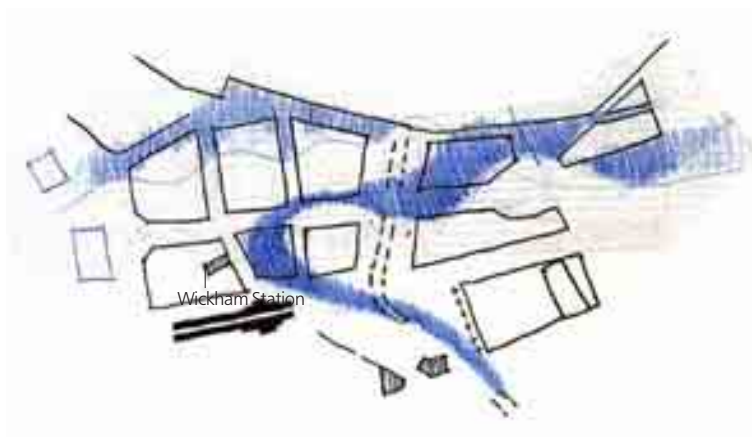
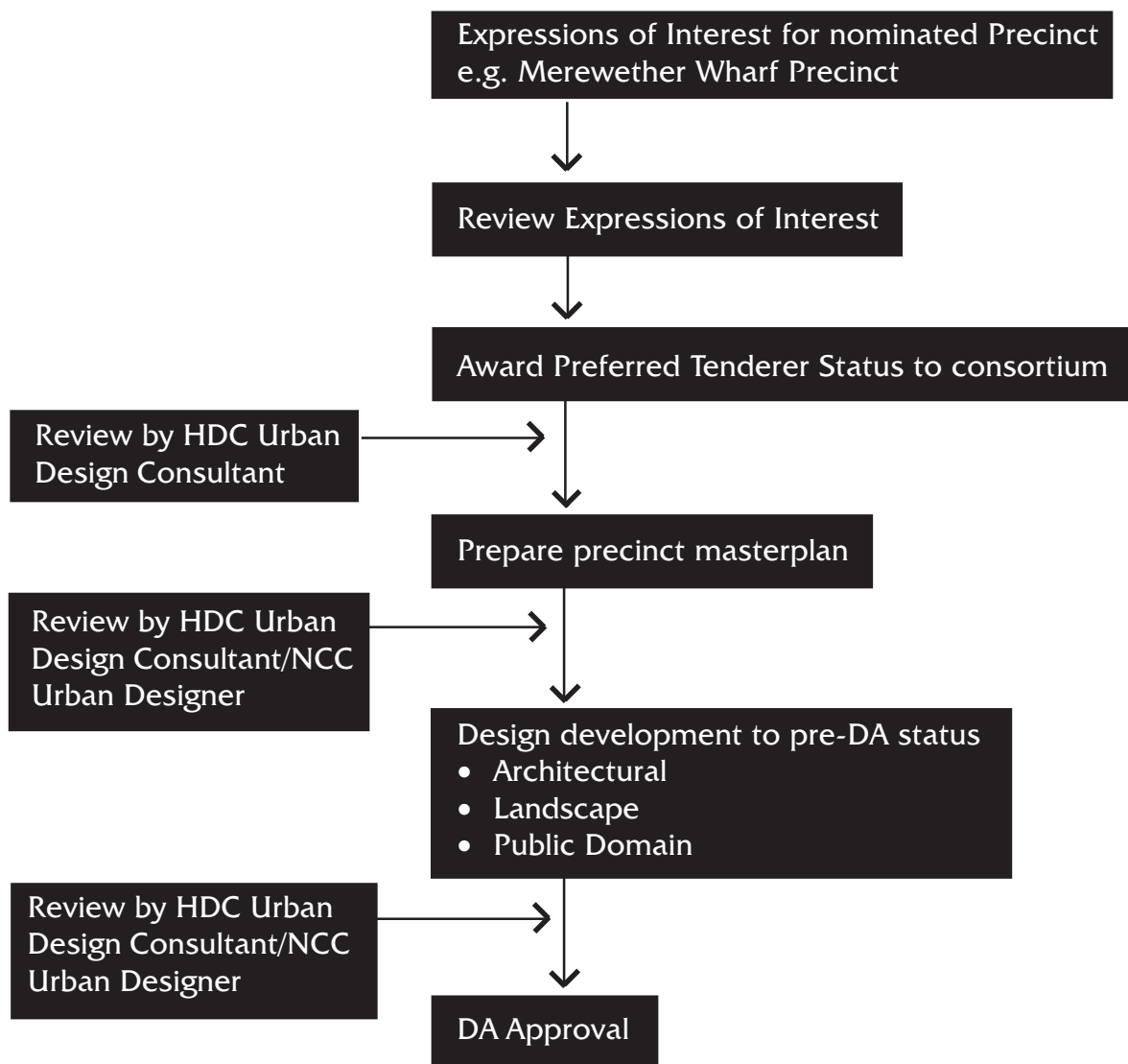


Diagram showing original Cottage Creek alignment

TYPICAL PUBLIC DOMAIN IMPLEMENTATION PROCESS



The Honeysuckle Public Domain will be developed incrementally over the next ten to fifteen years depending on market forces and local demand for the proposed development parcels. HDC have identified a sequential release of such development parcels commencing with the Merewether Wharf and Urban Entertainment Centre Precincts in the east (expressions of interest having closed at the time of writing) and moving westward toward Wickham. Associated with this central release area is an expression of interest to develop a multiplex cinema and retail complex adjacent to the Hunter Wine Society (west of squareabout). Simultaneously smaller development opportunities are being realised along the interface with City West as HDC release infill parcels along Hunter Street and Hannell Avenue.

Other more medium term development opportunities include the residential development along the Lee Wharf foreshore (Worth Place Precinct), the Cottage Creek Precinct and Wickham Urban Village. The timing of the latter will largely be determined by the program for the construction of the Hannell/Stewart Avenue intersection. This critical urban intervention will also facilitate the realignment of Honeysuckle Drive through to Hannell Avenue, thereby allowing the restructuring of the Wickham Station Precinct into the proposed urban village. Likewise the program for the reorganisation of the Civic connection through to the UEC is dependent on the future restructuring of the railway connection between Hamilton and Newcastle stations (i.e. heavy or light rail network). It is important to note that the implementation of essential elements, that combined will create the envisaged public domain at Honeysuckle, will be the responsibility of HDC in connection with Newcastle City Council and other government agencies.

Whilst the core fabric of the public domain will be incrementally stitched into place over time, as the various development precincts come on line, the strategy has identified several strategic public domain projects as being key to stimulating the necessary momentum to sustain development over the coming years. Those strategic public domain projects should be prioritised and budgeted for implementation over the next five years and include:

- 1 Worth Place Park and Foreshore linkages
- 2 Urban Entertainment Centre environs
- 3 Honeysuckle Drive
- 4 North South connectors
- 5 Cottage Creek

.

Worth Place Park and associated foreshore linkages will represent a significant commitment by HDC to providing a quality and accessible public domain and foreshore open space connection between Newcastle East and Linwood/Throsby Creek. This project would facilitate public access to the foreshore prior to any adjacent private sector development and ensure a degree of public ownership of the foreshore. Conversely, this open space infrastructure will serve as a seed project to both stimulate and potentially yield a higher standard of residential development, whilst establishing the character base for surrounding buildings and spaces

Whilst **Honeysuckle Drive** is accessible along its entirety and partially landscaped to final alignment between Worth Place and the UEC, significant work is required to create the contrasting riparian and boulevard landscape of the west and east sections proposed in the strategy. The program for the western section is dependent on the Hannell/Stewart Avenue intersections and Cottage Creek works. Should funding permit, the remaking of Honeysuckle Drive east could be undertaken independently with new street tree plantings and footpath pavements creating a more urbanised streetscape. Further the development and installation of temporary sites specific urban artworks would begin to sow the seeds of the Honeysuckle Story adjacent to the boulevard and create an early engaging urban dialogue.

The **connection** of City West through to the foreshore across Honeysuckle Drive is a primary objective of all key stakeholders. Whilst some of the connectors are dependent on associated urban interventions such as remaking Cottage Creek, Hannell/Stewart Avenue and reorganising Civic Station and closing Merewether Street, there is an opportunity to develop the Steel Street and Hunter Water connectors. The lead time for these central connectors should be brought forward to commence the feasibility and planning phase. As with Worth Place Park the development of the public domain along these connectors will facilitate public access where it was previously denied and potentially stimulate development along the connectors in line with the DCP 40 objectives.

The remaking of **Cottage Creek** will require significant resources, energy and collaboration between key stakeholders to achieve its realisation. However, it will represent a benchmark project that will reinforce Newcastle's reputation as a city committed to sustainable placemaking. Therefore the timing of this project is critical both in terms of lead time for project planning, design and documentation and actual construction time.

The implementation of these key public domain projects will prove to be extremely significant in a number of ways. Politically they will represent a major step forward in the development of an integrated place by the harbour – *‘something is finally happening’*. Socially the community will gain access to the foreshore from City West and develop an enthusiasm and sense of ownership for this place by the harbour. Environmentally the urban systems will be renewed as hybridised forms and new processes cleanse the urban waterways and provide habitat for birds and fish. Economically the provision of key public domain infrastructure should be seen as important as roads and services to attract appropriate development outcomes and achieve bottom line returns on the disposal of this prime urban development land. Essentially the implementation of key public domain projects and a quality integrated public domain represents the opportunity for a win-win situation

In closing it should be noted that a lively and activated public domain requires ongoing maintenance, management and repair. Organisations such as the NCC, Port Corporation and Hunter Water Corporation must be aware of this requirement and make adequate provision in their budgets for the ongoing maintenance of these areas.

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